

**CITY OF BERKLEY PUBLIC NOTICE
REGULAR CITY COUNCIL MEETING
Monday, June 2, 2025
7:00 P.M. – City Hall
248-658-3300**

**CALL 40th COUNCIL TO ORDER
APPROVAL OF AGENDA
MAYOR-LED MOMENT OF REFLECTION
PLEDGE OF ALLEGIANCE
PUBLIC COMMENT**

Comments are invited on each Agenda item when that item comes up for consideration. Matters not listed on the Agenda may be addressed at this time. Please state your name and residential city. Each speaker's remarks are a matter of public record, and the Council will not engage in a back-and-forth discussion. Any person speaking at a City Council Meeting may be called to order by the Mayor or any Council Member for failure to be germane to the business of the City or for disruptive or disorderly behavior which prevents the Council from conducting its business. There is a three-minute limit per speaker.

ORDER OF BUSINESS

Consent Agenda

1. **APPROVAL OF THE MINUTES:** Matter of [approving the minutes](#) of the 40th Regular City Council meeting on Monday, May 19, 2025.
2. **MOTION NO. M-39-25:** Matter of [declaring items as surplus](#).
3. **MOTION NO. M-40-25:** Matter of [approving the submission](#) of a SEMCOG Planning Grant Application for Coolidge Highway.
4. **MOTION NO. M-41-25:** Matter of [considering the second reading](#) and approval for an amendment to the Planned Unit Development previously approved for a multiple-family development on the east side of Coolidge Highway, south of Cambridge Road and north and south of Columbia Road, by the applicant, Krieger Klatt Architects on behalf of WJ Ventures LLC, 2465 Columbia, 2475 Columbia, 2468 Columbia, and 2476 Columbia and 2475 Cambridge.
5. **PROCLAMATION NO. P-22-25:** Matter of [receiving a proclamation](#) honoring the dedication and service of John "Jack" Blanchard to the City of Berkley.

Regular Agenda

1. **RECOGNITIONS/PRESENTATIONS:** Matter of receiving any recognitions or presentations from the Consent Agenda.
2. **MOTION NO. M-42-25:** Matter of [approving a Charitable Solicitation Permit application](#) submitted by Disarmory Ministries to destroy firearms every Friday and Saturday from 10 a.m.- 2 p.m. now through November 1, 2025 in the parking lot of Greenfield Presbyterian Church, 2312 Greenfield Road. The organization will have a donation bucket placed on the property.
3. **MOTION NO. M-43-25:** Matter of [approving a dedication ceremony](#) in honor of former Mayor Maybelle Fraser, and the purchase of a plaque to be placed on the City gazebo in her honor.
4. **MOTION NO. M-44-25:** Matter of [reviewing the Quarter 1 Strategic Framework report](#).

COMMUNICATIONS

ADJOURN

Note: The City of Berkley will provide necessary reasonable auxiliary aids and services, such as signers for the hearing impaired and verbal representations of printed materials being considered at the meeting, to individuals with disabilities at the meeting upon four working days' notice to the City. Individuals with disabilities requiring auxiliary aids or services should contact the City by writing or calling: Victoria Mitchell, ADA Contact, Berkley City Hall, 3338 Coolidge Highway, Berkley, MI 48072 (1-248-658-3310).

Note: Official minutes of City Council Meetings and supporting documents for Council packets are available for public review in the City Clerk's Office during normal working hours. Anyone wishing to submit correspondence to the Council before the meeting may send an email to comment@berkleymi.gov by noon on the day of the meeting. Emails sent prior to the deadline will be a part of the meeting record but will not be read during the Council meeting.

THE REGULAR MEETING OF THE FORTIETH COUNCIL OF THE CITY OF BERKLEY, MICHIGAN WAS CALLED TO ORDER AT 7 PM ON MONDAY, MAY 19, 2025 BY MAYOR DEAN

PRESENT: Councilmember Steve Baker
Mayor Pro Tem Ross Gavin
Councilmember Jessica Vilani
Mayor Bridget Dean
Councilmember Clarence Black
Councilmember Dennis Hennen

ABSENT: Councilmember Gregory Patterson

OTHER STAFF PRESENT:

City Manager Crystal VanVleck
City Attorney Dan Christ
City Clerk Victoria Mitchell
Deputy City Manager of Public Services Shawn Young
Community Development Director Kristen Kapelanski
Deputy Finance Director Amy Zurawski
Interim Downtown Development Authority Executive Director Jennifer Finney
Emergency Manager Jack Blanchard

APPROVAL OF AGENDA

Mayor Pro Tem Gavin moved to approve the Agenda
Seconded by Councilmember Hennen
Ayes: Black, Gavin, Hennen, Vilani, Baker and Dean
Nays: None
Absent: Patterson
Motion Approved.

Councilmember Black stated he is an officer of the Army and will be leaving us for deployment to an undisclosed location. It is the highest honor to lead the men and women of the U.S. Army. They are going to harm's way, so prayers are welcome, but don't feel bad for him, this is what he is trained for and what he loves to do. He will deploy for nine months to a year, but Public Safety does this every day of the year. He intends to be a contributing member of the Council, but he doesn't know. This will be his second-to-last meeting. He and his wife have four wonderful teenagers that she has to deal with, and maybe he gets the best end of the deal. He stated that his wife has been doing this for 20+ years, although she understands, please support her. He is going to miss the majority of his kids' senior year. He looks forward to coming back safely and serving.

Mayor Dean said he will be in all that prays' hands.

PUBLIC COMMENT

Muthu Veerappan, Big Law LLC, Auburn Hills, introduced himself to Council and stated that he had submitted a proposal for attorney services.

CONSENT AGENDA

Councilmember Baker moved to approve the following Consent Agenda
Seconded by Councilmember Black

APPROVAL OF THE MINUTES: Matter of approving the minutes of the 40th Regular City Council meeting on Monday, May 5, 2025; the Special Work Session on Monday, May 5, 2025; and the Special City Council Budget Work Sessions on Monday, April 28, 2025 and Tuesday, April 29, 2025.

WARRANT LIST: Matter of approving Warrant List No.1410.

ORDINANCE NO. O-06-25: Matter of approving the Second Reading and Adoption of an ordinance of the City Council of the City of Berkley, Michigan to add Article III, Workforce Housing PILOT to Chapter 114 Taxation, of the City of Berkley Code of Ordinances to provide for approval of an exemption from ad valorem property taxes for housing being developed or rehabilitated for workforce housing for persons and families whose household income is not greater than 120% of area median income, as authorized by provisions of the State Housing Development Authority Act of 1966, Public Act 346 of 1966. MCL 125.1401, et seq.

RESOLUTION NO. R-06-25: Matter of approving a resolution of the Council of the City of Berkley, Michigan authorizing the Decertification of Local Roadways.

MOTION NO. M-32-25: Matter of approving the Southeastern Oakland County Public Works Association joint and cooperative agreement.

RESOLUTION NO. R-07-25: Matter of approving participation in the "Tie Michigan Teal" campaign to promote awareness of ovarian cancer and its symptoms.

PROCLAMATION NO. P-19-25: Matter of proclaiming June 2025 as LGBTQIA+ Pride Month.

PROCLAMATION NO. P-20-25: Matter of proclaiming June 19, 2025 as Juneteenth Celebration Day.

PROCLAMATION NO. P-21-25: Matter of proclaiming June 6, 2025 as National Gun Violence Awareness Day.

Ayes: Gavin, Hennen, Vilani, Baker, Black and Dean
Nays: None
Absent: Patterson
Motion Approved.

REGULAR AGENDA

RECOGNITIONS/PRESENTATIONS: Matter of receiving any recognitions or presentations from the Consent Agenda.

Councilmember Vilani read P-19-25 in its entirety.
Councilmember Black read P-20-25 in its entirety.

MOTION NO. M-33-25: Matter of considering the First Reading for an amendment to the Planned Unit Development previously approved for a multiple-family development on the east side of Coolidge Highway, south of Cambridge Road and north and south of Columbia Road by the applicant, Krieger Klatt Architects on behalf of WJ Ventures LLC, 2465 Columbia, 2475 Columbia, 2468 Columbia, 2476 Columbia, and 2475 Cambridge.

Councilmember Black moved to approve Motion No. M-33-25

Seconded by Mayor Pro Tem Gavin

Ayes: Hennen, Vilani, Baker, Black, Gavin and Dean

Nays: None

Absent: Patterson

Motion Approved.

Wayne Wudyka, Birmingham, introduced changes made to the project since it was originally approved.

Ray Phillips, Krieger Klatt, went through the presentation reviewing all of the changes that Director Kapelanski discussed previously.

MOTION NO. M-34-25: Matter of approving the “Berkley Street Art Fest” event on Saturday, July 12, 2025 from 11 a.m.-4 p.m. on Coolidge Highway, between Catalpa and Beverly, for the Berkley Area Chamber of Commerce, P.O. Box 72-1253. Approval is conditional upon the submission of required items and documents prior to event dates.

Councilmember Vilani moved to approve Motion No. M-34-25

Seconded by Mayor Pro Tem Gavin

Ayes: Vilani, Baker, Black, Gavin, Hennen and Dean

Nays: None

Absent: Patterson

Motion Approved.

MOTION NO. M-35-25: Matter of approving the “Berkley Art Bash” event on Saturday, June 14, 2025 from 10 a.m.-6 p.m. on 12 Mile Road for the Berkley Area Chamber of Commerce, P.O. Box 72-1253. Approval is conditional upon the submission of required items and documents prior to event dates.

Councilmember Vilani moved to approve Motion No. M-35-25

Seconded by Councilmember Black

Ayes: Vilani, Baker, Black, Gavin, Hennen, and Dean

Nays: None

Absent: Patterson

Motion Approved.

MOTION NO. M-36-25: Matter of approving a Downtown Development Authority (DDA) event, "Berkley Pride Block Party" on Robina south of 12 Mile Road, on Gardner south of 12 Mile Road, and in the Municipal Parking Lot located between those two roads, on Sunday, June 22, 2025 from 1 to 4 p.m. Approval is conditional upon the submission of required items and documents prior to the event date.

Councilmember Baker moved to approve Motion No. M-36-25

Seconded by Councilmember Vilani

Ayes: Baker, Black, Gavin, Hennen, Vilani, and Dean

Nays: None

Absent: Patterson

Motion Approved.

PRESENTATION: Matter of receiving a presentation regarding the 2025/26 FY City Manager proposed budget for the City of Berkley, Michigan.

Crystal VanVleck opened the presentation. She introduced Amy Zurawski, Deputy Finance Director.

Ms. VanVleck said the presentation is an overview and the budget document goes into greater detail which is available on the City's website.

PUBLIC HEARING: Matter of holding a public hearing regarding the City Manager proposed 2025/26 FY Budget and supporting tax rates for the City of Berkley, Michigan.

Public Hearing Opened: 8:58:13

Public Hearing Closed: 8:58:21

RESOLUTION NO. R-08-25: Matter of approving a resolution of the Council of the City of Berkley, Michigan approving the 2025/26 through 2030/31 Capital Improvement Plan of the City of Berkley, Michigan.

Mayor Pro Tem Gavin moved to approve Resolution No. R-08-25

Seconded by Councilmember Vilani

Ayes: Black, Gavin, Hennen, Vilani, Baker and Dean

Nays: None
Absent: Patterson
Motion Approved.

RESOLUTION NO. R-09-25: Matter of approving a resolution of the Council of the City of Berkley, Michigan setting the residential and commercial service fee for the collection and disposal of solid waste, effective July 1, 2025.

Councilmember Hennen moved to approve Resolution No. R-09-25

Seconded by Councilmember Vilani

Ayes: Gavin, Hennen, Vilani, Baker, Black and Dean

Nays: None

Absent: Patterson

Motion Approved.

RESOLUTION NO. R-10-25: Matter of approving a resolution of the Council of the City of Berkley, Michigan setting the various rates for water and sewer services, effective July 1, 2025.

Mayor Pro Tem Gavin moved to approve Resolution No. R-10-25

Seconded by Councilmember Baker

Ayes: Hennen, Vilani, Baker, Black, Gavin and Dean

Nays: None

Absent: Patterson

Motion Approved.

MOTION NO. M-37-25: Matter of receiving updates to the City of Berkley Fee Schedule.

Councilmember Hennen moved to approve Motion No. M-37-25

Seconded by Councilmember Black

Ayes: Vilani, Baker, Black, Gavin, Hennen and Dean

Nays: None

Absent: Patterson

Motion Approved.

RESOLUTION NO. R-11-25: Matter of approving a resolution of the Council of the City of Berkley, Michigan adopting the annual budget for the fiscal year 2025/26 and acknowledgment of the 2026/27 and 2027/28 budget projections.

Councilmember Baker moved to approve Resolution No. R-11-25

Seconded by Mayor Pro Tem Gavin

Ayes: Vilani, Baker, Black, Gavin, Hennen and Dean

Nays: None

Absent: Patterson

Motion Approved.

MOTION NO. M-38-25: Matter of approving the terms of the second amendment to the City's Ground Lease agreement with AT&T.

Councilmember Hennen moved to approve Motion No. M-38-25

Seconded by Mayor Pro Tem Gavin

Ayes: Baker, Black, Gavin, Hennen, Vilani and Dean

Nays: None

Absent: Patterson

Motion Approved.

COMMUNICATIONS:

COUNCILMEMBER BLACK

- Reported that the Committee for Engagement and Transparency is working on a comprehensive survey to determine how residents would like to receive communications.
 - Noted that there was no meeting this month, so more information will follow.

COUNCILMEMBER HENNEN

- The Tree Board will meet on Tuesday, May 27th at 7 PM in the second-floor Public Safety Conference Room.
- The Zoning Board of Appeals has no upcoming meetings.
- Shared that he attended the Michigan Municipal League Summit on Leadership over the weekend and highly recommended it; he was unexpectedly asked to speak at the summit about Berkley's early look at the MML's Thriving Communities Framework and its potential benefits. He reflected that his personal takeaway was the importance of not just sharing facts but also explaining the "why" behind them to be a better communicator. He stated that although he is analytical by nature, he intends to work on the emotional side of communication based on what he learned at the summit.

COUNCILMEMBER BAKER

- The Historical Committee met on Tuesday, May 13th. He wished everyone a happy Berkley History Month and encouraged visits to the museum. The museum has received a collection of Berkley High School band uniforms, including a mannequin fully dressed in one on display.
 - A Berkley-wide scavenger hunt will run from July 1st to August 31st, with more details forthcoming and great prizes planned.
 - Visit berkleyhistory.com for more information.
- The Downtown Development Authority (DDA) met on Wednesday, May 14th in the morning.
 - Shared news about the upcoming Dorothea Pocket Park Project pilot program, which will run from June 14th to July 19th and will close off that street for fun events and activities. He mentioned the project fulfills a long-standing item in the city's master plan for creating more public space.
 - Noted that the Michigan Municipal League will help coordinate a walking tour of the west end of 12 Mile Road in June to explore more placemaking opportunities.
 - Visit downtownberkley.com for more event information.
- Quoted Malcolm X: "We need more light about each other. Light creates understanding. Understanding creates love. Love creates patience. And patience creates unity." Malcolm X was born on May 19, 1925, along with Grace Jones and André the Giant, who was his favorite wrestler growing up.
 - Listed observances for the day: National Plant Something Day, World Family Doctor Day, Frog Jumping Jubilee Day, and National Elected Officials Day.
- Praised Councilmember Black, calling him an inspirational leader and a great representative for Berkley and the region, and wished him safe travels, a smooth deployment, and a happily uneventful return home.
- Encouraged everyone to hug somebody they love.

MAYOR PRO TEM GAVIN:

- Reported that the Parks and Recreation Advisory Board will meet June 12th at 7 PM in the Community Center.
- Announced that the Library Board will meet on May 21st at 7 PM at the Library.

COUNCILMEMBER VILANI:

- The Environmental Advisory Committee's Native Plant Sale will be from 10 AM to 2 PM on June 1st at the Community Center.
- The Berkley Area Chamber will hold a Chamber Chat with the Royal Oak Leprechauns on Coolidge at 8:30 AM on June 20th, open to all.
- Highlighted the upcoming Berkley Art Bash and encouraged people to vote in the Berkley Has Heart Photography Contest.

- Reminded everyone that the Street Art Festival is on July 12th and that chalk artist registration is still open.
- Thanked Councilmember Clarence Black, referring to him as both a colleague and a friend, and expressed her appreciation for his service on the Council and to the country.

CITY MANAGER VANVLECK:

- Encouraged community members and stakeholders to volunteer for Downtown Development Authority (DDA) committees, including Business Improvement, Design, Organization and Development, and Marketing and Promotion. She directed interested individuals to contact DDA Director Jennifer Finney.
- Provided an update on Human Resources: the HR Director has been busy filling vacant and seasonal positions due to retirements, internal moves, and resignations. She also mentioned the upcoming employee healthcare open enrollment period at the end of June and praised the HR Director's positive impact on the organization.
- Planning is underway for the Dorothea Pilot Pocket Park, scheduled from June 18th to July 19th. She shared that the street closure will coincide with the Art Bash weekend, and DPW will assist with logistics. Volunteers and summer camp kids will help design and paint the space, with a soft opening and kickoff likely on June 21st. She invited the public to submit creative ideas for programming in the park, and encouraged musicians or anyone with ideas to contact city staff.

CITY ATTORNEY CHRIST:

- Reported that the city's final Michigan Tax Tribunal matter was resolved the prior week. He explained that the property owner had been seeking a \$500,000 reduction in taxable value, but it was resolved for a much smaller amount. He stated that the resolution was achieved in coordination with Oakland County Equalization and that there are now no pending tribunal matters.

MAYOR DEAN:

- Shared that she attended the National League of Cities Risk Conference in Clearwater, Florida, as part of her duties as a trustee for the Michigan Municipal League Workers Comp Fund. She said she enjoyed one-and-a-half days of Florida weather before attending full-day conference sessions, while her husband enjoyed the entire stay. She expressed appreciation for the Michigan Municipal League for the opportunity.
- Explained that Council Member Patterson was absent because his mother is in ICU in Port Huron, though stable and expected to return home in a few days.
- Addressed Councilmember Black directly, acknowledging that he has done and can do hard things, and commended his service both to the community and the country. She reiterated that it was "see you later, not goodbye" and noted there would be time to connect before his departure.

ADJOURNMENT:

Councilmember Vilani moved to adjourn the Regular Meeting at 9:28 PM

Seconded by Councilmember Black

Ayes: Gavin, Hennen, Vilani, Baker, Black and Dean

Nays: None

Absent: Patterson

Motion Approved.

Bridget Dean, Mayor

ATTEST:

Victoria Mitchell, City Clerk

DRAFT

June 2, 2025 City Council Meeting

Moved by Councilmember _____ and seconded by Councilmember
_____ to declare items as surplus.

Ayes:

Nays:

Absent:

Motion:

MEMORANDUM

To: Mayor Dean and City Council
From: Shawn Young, Deputy City Manager for Public Services
Date: June 2nd, 2025
Subject: Declaring items as surplus

Madam Mayor and Members of City Council,

Background

The City of Berkley makes it a point to update and repurpose its aging equipment when possible. All of the attached equipment listed items has been offered to other departments and has determined to be no longer needed due to condition, age, resale value, etc.

Summary

- Prior to being recommended as surplus, all equipment and vehicles are offered for reutilization by other departments
- The main driver of surplus equipment is the total cost of ownership, which includes current usage, maintenance and repair costs, and resale value.
- All items will be sold via public auction or disposed of accordingly.

Recommendation

It is my recommendation that the attached items be declared as surplus to be sold or disposed of accordingly.

1. DPW vehicle # 9. 2016 Ford F250 VIN # 1FT7W2B67GEB43311
Vehicle was replaced and is considered a surplus. No problems with vehicle.



2. DPW vehicle # 3. 2017 Ford F250 VIN # 1FT7X2B64HEF21132.
Vehicle was replaced and is considered a surplus. No problems with vehicle.



3. DPW vehicle # 8. 2017 Ford F250 VIN # 1FTBF2B63HEF21118.
Vehicle was replaced and is considered a surplus. No problems with vehicle.



4. DPW vehicle # 22. 2016 Elgin Pelican Street Sweeper SER # E0081000001
Sweeper was replaced and considered a surplus. No problems with sweeper.



5. Public Safety vehicle # 412. 2019 Ford Explorer Police UV VIN # 1FM5K8AR3KGA30689
Vehicle was replaced and is considered a surplus. No problems with vehicle.



June 2, 2025 City Council Meeting

Moved by Councilmember _____ and seconded by Councilmember
_____ to approve the submission of a SEMCOG Planning Grant Application for
Coolidge Highway.

Ayes:

Nays:

Absent:

Motion:

MEMORANDUM

To: Mayor Dean and City Council
From: Shawn Young, Deputy City Manager for Public Services
Date: June 2nd, 2025
Subject: Recommendation of Submission Approval- SEMCOG Planning Grant Application

Madam Mayor and Members of City Council,

Background

As part of our overall Capital Outlay Planning we are looking at the potential for a Coolidge Hwy Project. A big part of this project is defining a future vision for the Coolidge Corridor. We would like to pursue a grant opportunity through SEMCOG to help us realize this vision.

Summary

- This purposed grant would be a request from SEMCOG of \$36,832.50 and would require the City to provide matching funds of \$8,167.50 for a total of \$45,000 to be used for the initial planning portion of the project.
- This type of planning would hep us realize our scope of work. Once we have this we can look into more creative funding opportunities.
- This grant would cover the initial business and community outreach, public workshops, coordination with our community partners, and any other planning activities.
- Information obtained would be used to create a robust and engaged plan with recommendations ultimately be submitted to council for feedback before moving on with further designs.

Recommendation

- It is my recommendation that City Council approves submittal of the SEMCOG Planning Grant Application for Coolidge Hwy.

May 15, 2025

Kevin Vettrano, Director of Planning
SEMCOG
1001 Woodward Ave, Suite 1400
Detroit, MI 48226

Re: FY2026 SEMCOG Planning Assistance Grant – City of Berkley

Dear Mr. Vettrano,

The Berkley Downtown Development Authority (DDA) is writing in support of the City of Berkley's application for a FY2026 Planning Assistance Grant to create the "Community Visioning Plan for Coolidge Highway from 11 Mile Road to 12 Mile Road". In 2019, the DDA partnered with the City of Oak Park and City of Huntington Woods on a Multi-Community Planning: Eleven Mile Road and Coolidge Highway Study. It was a technical guidance document to assist the three communities with a design framework for future infrastructure improvements. The report provided a recommendation for the City of Berkley to implement a road diet on Coolidge Highway. That pilot project was completed in 2021.

Berkley is ready for the next phase of improvements and would like to implement a public engagement strategy to solicit and present more specific ideas that will be centrally focused on the Berkley community and stakeholders. The city would like open dialogue from all demographic groups, including the business community, regarding potential streetscape improvements, green infrastructure, safety, and non-motorized infrastructure.

The Berkley DDA's mission is to be a community-driven organization striving to enhance the shopping experience, economic vitality and physical appearance of Coolidge Highway and Twelve-Mile Road – Berkley's traditional commercial Districts. We believe the City's public engagement efforts to create a Community Vision Plan for the Coolidge Highway segment support our mission, and their ideas to engage the business community support our vision to make Berkley a compelling place to work, live, and play.

Sincerely,

Jennifer Finney

Jennifer Finney, Interim Director
Berkley DDA



SCOTT FRANCIS

Superintendent

14501 Talbot, Oak Park, MI 48237

p. 248-837-8004 f. 248-837-8065

www.berkleyschools.org

May 16, 2025

Kevin Vettraino, Director of Planning
SEMCOG
1001 Woodward Ave, Suite 1400
Detroit, MI 48226

Re: FY2026 SEMCOG Planning Assistance Grant – City of Berkley

Dear Mr. Vettraino,

Berkley Schools is writing in support of the City of Berkley's application for a FY2026 Planning Assistance Grant to create the "Community Visioning Plan for Coolidge Highway from 11 Mile Road to 12 Mile Road".

Berkley would like to implement a public engagement strategy to solicit and present more specific ideas for improvements that will be centrally focused on the Berkley community and stakeholders. The city would like open dialogue from all demographic groups, including Berkley High School students, regarding potential streetscape improvements, green infrastructure, safety, and non-motorized infrastructure. This will give students an opportunity to see government in action.

Berkley Schools operates under guiding principles including the following District Vision and District Mission Statements: "Our inclusive culture will inspire, empower and lead all learners to realize their full potential and make a positive difference in their future and community." and "Student engagement, learning and growth are at the center of all we do." We believe the City's public engagement efforts to create a Community Vision Plan for the Coolidge Highway segment support our vision and mission, and their ideas to engage with the students will make a positive difference in their future and community.

Sincerely,

Scott Francis, Superintendent
Berkley Schools

Berkley's SEMCOG Planning Assistance Program Application

- 1. Community/Agency Name:** City of Berkley
- 2. Project Primary Contact:** Shawn Young
- 3. Primary Contact Phone Number:** 248-658-3499
- 4. Primary Contact Email Address:** syoung@berkleymi.gov
- 5. Project Name:** Community Visioning Plan for Coolidge Highway from 11 Mile Road to 12 Mile Road
- 6. Project Type:** Streetscapes and Pedestrian Focused Placemaking Planning
- 7. Project Location and Limits (county,city/village/township, street name/cross streets, etc):** Coolidge Hwy, between 11 Mile Road and 12 Mile Road
- 8. Project Partners and/or Communities/Agencies (list lead community first):**
City of Berkley, City of Berkley DDA, Berkley Chamber of Commerce, Berkley Public Library, Berkley High School
- 9. In a brief narrative, describe the proposed project and how it aligns with at least one of the eligible topics (Transportation Accessibility, Safety, and Multimodal Corridor Planning, Streetscapes and Pedestrian Focused Placemaking Planning, Multi-community Transit and Demand Response Public Transportation Planning, Trails and Greenways Planning, Stormwater Management Planning). Please include if the project is multi-community and/or how the project will benefit or could be a regional pilot for communities across Southeast Michigan (attach additional information, if necessary):**

The City of Berkley would like a SEMCOG Planning Assistance Grant to develop a Community Visioning Plan for Coolidge Highway from 11 Mile Road to 12 Mile Road. The requested grant funds will cover the expenses associated with public engagement and early planning as it relates to the future of this corridor. Expanding from a previous 2019 economic development corridor study conducted on Coolidge Highway and 11 Mile Road in cooperation with the City of Oak Park and the City of Huntington Woods, and a Complete Streets Pilot Project in 2021-2022, Berkley wishes to plan for the next phase of improvements by presenting more specific ideas that will be centrally focused on the Berkley community. By engaging with and soliciting feedback from several different forums and methods, the city can reach all demographic areas and encourage open, thoughtful, and productive dialogue from residents, the business community, patrons of the business district, and Berkley High School students. The feedback regarding potential streetscape improvements, green infrastructure, safety and non-motorized infrastructure will provide the city with a vision for the corridor and enhance future planning and design efforts. By engaging more stakeholders, the city can determine what is working and what needs improvement. The project aligns with SEMCOG's Streetscapes and

Pedestrian Focused Placemaking Planning initiative by providing the framework to improve access and connectivity for those who walk, bike or use transit. The Visioning Plan will also provide branding of the city's gateway to public services and business district. By adding green infrastructure, non-motorized friendly elements, and improving vehicular and vulnerable road user safety, the city will enhance opportunities for residents and visitors to socialize, relax, and enjoy the culture and identity of the Coolidge Highway Corridor district.

10. Describe the proposed process and/or approach to ensure this project will support and improve one or more of the five eligible topics (Transportation Accessibility, Safety, and Multimodal Corridor Planning, Streetscapes and Pedestrian Focused Placemaking Planning, Multi-community Transit and Demand Response Public Transportation Planning, Trails and Greenways Planning, Stormwater Management Planning):

The proposed process supports and improves Streetscapes and Pedestrian focused placemaking and Multimodal Corridor Planning by a series of public engagement initiatives that cover all stakeholders in Berkley. The engagement plan is comprised of the following:

Public Workshop: The City of Berkley will contract with a planning firm to host a public workshop/charrette that will be well publicized through traditional and social media. The workshop will be held near the Coolidge Highway Corridor and during the evening hours to maximize the number of attendees. The purpose of the workshop is to provide the public with a comfortable setting to discuss their preferences and opinions regarding this section of Coolidge and to develop a vision for the corridor. The firm will provide background, traffic, and safety information as well as visuals to provide context to the group. Large maps will be provided for participants to mark up their ideas. Open-ended comment sheets will be provided and collected at the workshop as well. The public workshop will be open to all residents, visitors, and various stakeholders such as the business community, disability advocates, transit users, Berkley School District, SEMCOG, and public services.

Business Engagement: The planning firm will work with the DDA and Chamber of Commerce to advertise and conduct in-person "walkabout" meetings with the businesses along and near Coolidge. Each meeting will be conducted in a time frame that allows business owners to discuss their observations, express concerns and provide their vision for the corridor. The in-person nature of these meetings encourages open dialogue and are beneficial to understanding roadway operations and determining potential design scope.

Youth Engagement: Berkley High School students are frequent users of Coolidge Highway and their perspective on current conditions is beneficial in developing the next phase of the corridor that is safe and enhances the quality of life for residents. The city's planning firm will work with Berkley High School administration to determine a time and forum to engage students in the development of a vision for Coolidge Highway. This forum allows a younger audience to experience local government in action and how their perspective about improvements may differ or echo the other stakeholders.

Open Public Comment Period: In sync with the in-person public engagement activities, the city's planning firm will develop an online survey that will be publicized on social media, and through the distribution of QR codes (perhaps in a local Berkley newsletter) for residents complete. The survey will provide the residents with the option to map specific areas of concern, or locations that would benefit from future improvements or features.

As an additional engagement effort, the city will coordinate with the Berkley Public Library and create a display with quick and easy feedback cards for patrons to provide their input and ideas.

Lastly, the city's planners will work with the DDA to find a storefront that can host a mural, map, or information about the Visioning project. A mural could be painted by the high school art classes or clubs, further engaging students in the government process. This initiative is tentative and conditioned upon the availability of a storefront and the availability of students to participate.

11. Other project considerations (this may include innovations or planning approaches that can be replicated by other communities):

Additional Innovative Engagement Efforts: The city's planning firm will coordinate with the DDA to provide markable drink coasters at Berkley restaurants asking patrons to write their ideas for improvements. This is a unique way to engage with patrons of the corridor and will help us gather an outside perspective from those that contribute to the local economy. The city and DDA will give participants an option to provide their contact information and coordinate a gift card drawing with the DDA. This approach can be replicated by other communities.

12. Total Request from SEMCOG (81.85% of total project cost): \$36,832.50

13. Source(s) of Match: City of Berkley Local Cash Match of \$8,167.50

14. Project Schedule (implementation dates & activity milestones):

Early August 2025: Kick-off Meeting, Public Engagement Plan and Coordination

August 13, 2025: DDA Meeting at 8:15am – 9:45am. Berkley Public Safety Building 2nd Floor, Conference Room.

September 2025: Online survey open and available to complete at any time. It will close a week after the public workshop.

Late September 2025: Business Owner Walkabouts

Late September/Early October 2025: Berkley High School Public Engagement

September/October 2025: Restaurant Patron Public Engagement (Full 30 days minimum)

September/October 2025: Berkley Public Library Patron Public Engagement (Full 30 days minimum)

Late October 2025: Public Workshop & Work with DDA to find appropriate storefront mural location

November 1-30, 2025: Data analyzation & Draft Plan recommendations

December 15, 2025 – Plan Recommendations presented to Berkley City Council

15. Explain public input received in shaping this project concept. Include any partners or stakeholders who will assist in implementing this project:

Public input was received during the creation of the 2021 Berkley Master Plan. The community engagement element was conducted from 2019-2021 and included public meetings (prior to the pandemic), interviews, focus groups, community surveys, webinars, walking tours, and virtual open houses. The residents indicated preferences for commercial, entertainment, walkability, bike ability, and placemaking in their corridors, including Coolidge Highway. Residents also expressed more interest in green initiatives and implementing green infrastructure on commercial and municipal properties and in streetscape improvements. Input was also received in 2018 when the City adopted a Complete Streets Ordinance, with Coolidge Highway as one of the targeted corridors.

In 2021, the City of Berkley initiated the Coolidge Highway Pilot Project. Public and Departmental feedback from that project has led to the determination that additional stakeholder and public feedback is necessary to develop a Coolidge Highway Corridor that meets the needs of the community. In the attached "Coolidge Complete Streets Pilot Project Recommendations" report, the Department of Public Safety recommends determining the feasibility of multi-modal facilities, parking and traffic operations. Working with the school district is necessary to gather input on student and faculty safety, identify concerns and develop recommendations for future corridor improvements. In addition, the feedback from the pilot project has encouraged the city to facilitate input from residents of neighboring communities to explore their commercial corridors as current or potential future customers of Berkley businesses. The DDA also provided a list of recommendations in the report, including various crosswalk improvements, the use of elevated berms, pedestrian safety signage, and education and outreach to the public. Based on the recommendations in the report, the DDA, School District, Chamber of Commerce, SMART, and the Berkley Public Library will be invited to participate as stakeholders and assist with the implementation of the city's public engagement efforts to develop the Visioning Plan.

16. Regional or State Plan which this project supports:

- **Economic Development Strategy for Southeast Michigan**
- **Bicycle & Pedestrian Mobility Plan for Southeast Michigan**
- **Green Infrastructure Vision for Southeast Michigan**
- **Water Resources Strategy for Southeast Michigan**
- **Vision 2050 Regional Transportation Plan for Southeast Michigan**
- **Traffic Safety Plan for Southeast Michigan**
- **Other (please specify): Increasing Shared Prosperity for a Resilient Economy**

17. Explain how this project is consistent with the above plan(s):

The Community Visioning Plan for Coolidge Highway is consistent with SEMCOG's Bicycle & Pedestrian Mobility Plan, Green Infrastructure Vision, the Vision 2050 Plan, and the Increasing Shared Prosperity for a Resilient Economy Plan. The corridor and potential improvements fit within the Bicycle & Pedestrian Mobility Plan; the city's public engagement plan is consistent with the Green Infrastructure Vision because it includes collaborating with government agencies, regional partners, and the public to coordinate and communicate the benefits of

green infrastructure and promotes best practices. The city's project and subsequent plan is consistent with the Shared Prosperity and Equity elements of Vision 2050 by promoting accessible multi-modal transportation to natural and cultural resources that support opportunities for recreation, tourism, and local business as well as improving the city's comprehensive active transportation system that meets the needs of people of all ages and abilities. Lastly, the city's project is consistent with the Increasing Shared Prosperity for a Resilient Economy Plan by creating and marketing quality spaces to support efforts to develop local neighborhoods and business districts as dynamic and diverse places to live, work and visit and by maintaining, and expanding multi-modal transportation options to safely and efficiently connect people & places, and bolster community engagement.

- 18. I agree to the administrative requirements listed in the Frequently Asked Questions document, including the provision of the required 18.15% match:**

☐

Yes – I agree

June 2, 2025 City Council Meeting

Moved by Councilmember_____ and seconded by Councilmember _____ to consider the second reading and approval for an amendment to the Planned Unit Development previously approved for a multiple-family development on the east side of Coolidge Highway, south of Cambridge Road and north and south of Columbia Road by the applicant, Krieger Klatt Architects on behalf of WJ Ventures LLC, 2465 Columbia, 2475 Columbia, 2468 Columbia, 2476 Columbia, and 2475 Cambridge.

Ayes:

Nays:

Absent:

Motion:

MEMORANDUM

To: Mayor Dean and City Council
From: Kristen Kapelanski, Community Development Director
Date: June 2, 2025
Subject: Amendments to the Planned Unit Development for The Columbia

Madam Mayor and Members of City Council,

The applicant has submitted an amendment to the approved Planned Unit Development for The Columbia. The project history is detailed in the Community Development Department review memo.

Background

- The applicant has submitted an amendment to the approved Planned Unit Development for The Columbia. The project history is detailed in the Community Development Department review memo.
- **Planned Unit Development Amendment Process:**
 - A Planned Unit Development (PUD) is intended to offer an alternative to traditional development by permitting flexibility in the regulations for development while maintaining a consistent implementation of the goals and objectives of the City's Master Plan. Major amendments to the plan and any amendments necessitating alterations to or new ordinance deviations must follow the PUD approval process.
 - The Planning Commission is required to hold a public hearing and either recommend approval, recommend denial or postpone consideration of the PUD. Public hearing notices were sent via regular mail and posted in the newspaper in accordance with City and State requirements. Signs were also posted on the subject properties.
 - If/when the Planning Commission recommends approval or denial, the PUD proceeds to the City Council for their consideration for a first and second reading. The applicant is obligated to construct the plan as shown in the approved PUD and as outlined in the PUD Agreement.

Summary

- **Plan Changes:** A Planned Unit Development for this site was approved in 2024 by the City Council, after a positive recommendation from the Planning Commission. The applicant is proposing the following changes as part of the proposed amendment:
 - The southern building has been eliminated and all units have been consolidated into one building on the northern-most parcel. The pocket park originally on the eastern half of the southern parcel has been moved to the west, fronting Coolidge. Carports are proposed as accessory structures on the southern parcel.

- Conversion of the mansard roof into a flat deck roof. Height per the Zoning Ordinance is measured to the mid-point of a mansard roof and the top of a flat roof. In the original PUD, the top of the mansard roof was labeled as 45 ft. with a midpoint of 40 ft., the maximum allowed height. Since a flat top roof is measured differently, with a proposed height of 46 ft., this will require a deviation. Parapets are also proposed for visual interest and screening. Those are not included in the overall height measurements. See the applicant's February 14, 2025 narrative for additional information.
- The first-floor lobby has been increased in size.
- The unit count has decreased from 57 to 51 units.
- Balcony locations have been added and adjusted.
- The façade has been updated to have similar materials but with a different color palate. Cement board siding has been added as an accent material on the east, north and south elevations. Siding can be permitted by the Planning Commission if the Commission finds that the standards in Section 138-678 have been met.
- The parking lot layout has been revised and the number of parking spaces has been reduced from 77 to 69. With the reduction of units, this equals a ratio of 1.35 spaces per unit. The current approved plan also proposed 1.35 spaces per unit. The parking lot reconfigurations result in a more usable parking area without the use of tandem spaces.
- Landscaping throughout the site has been adjusted to accommodate the proposed layout changes.
- The refuse area has been moved under the podium of the building.
- It is important to note that the items identified by the Planning Commission and City Council as public benefits in the previous iteration have not been significantly changed. The question posed to the Commission and the Council with this new submission is whether the requested deviations (which have been slightly altered) do not outweigh the previously recognized public benefits.
- Deviations that are proposed as part of the PUD plan are listed below. New deviations are bolded. Revised deviations are italicized.
 - Deficient use (Non-residential is not provided on the first floor.)
 - ***Deficient use (The southernmost parcel contains a public park space/plaza and parking for the proposed multifamily building. Parking and a park are not permitted uses in the Gateway Zoning District.)***
 - *Deficient front setback -west (10 ft. required, 0.5 ft. provided)*
 - ***Deficient building height (40 ft. required, 46 ft. provided)***
 - *Deficient number of parking spaces (102 required, 69 provided)*
 - *Deficient number of compact car spaces (30% permitted, 35% proposed)*
 - ***Deficient use (Accessory structure – carports- located on a parcel without a principal use.)***
 - ***Deficient parking location (Off-street parking facilities must be located on the same parcel as the principal use. Parking proposed on southern parcel.)***
- The Planning Commission held the required public hearing and recommended approval of the amendments to the PUD plan on March 18, 2025. At that meeting they included a condition that the applicant work with staff to better activate the proposed park space. The applicant has included a walking path and landscape alterations to more clearly show that space as more than open grass.

- The City Council approved the first reading of the PUD amendment on May 19, 2025.

Recommendation

Moved by Councilmember _____ and seconded by Councilmember _____ to approve the second reading of the proposed amendment to PUD-01-25 The Columbia with the following ordinance deviations:

- Deficient use (Non-residential is not provided on the first floor.)
- Deficient use (The southernmost parcel contains a public park space/plaza and parking for the proposed multifamily building. Parking and a park are not permitted uses in the Gateway Zoning District.)
- Deficient front setback - west (10 ft. required, 0.5 ft. provided)
- Deficient building height (40 ft. required, 46 ft. provided)
- Deficient number of parking spaces (102 required, 69 provided)
- Deficient number of compact car spaces (30% permitted, 35% proposed)
- Deficient use (Accessory structure – carports- located on a parcel without a principal use.)
- Deficient parking location (Off-street parking facilities must be located on the same parcel as the principal use. Parking proposed on southern parcel.); and

With the following findings:

- The minimum of three of the seven public benefits listed in Section 138-533 have been met including:
 - That the project provides extensive landscaping, beyond the site plan requirements;
 - That the project provides open space or a public plaza; and
 - That the project provides efficient consolidation of poorly dimensioned parcels; and
- The standards for approval in Section 138-537 are met.

“THE COLUMBIA” AMENDED PLANNED UNIT DEVELOPMENT AGREEMENT

THIS AMENDED PLANNED UNIT DEVELOPMENT AGREEMENT (together with its Exhibits, the **“Amended Agreement”** or **“Amended PUD Agreement”**) is made as of March_____, 2025, by and between, WJR Ventures, LLC, a Michigan limited liability company (the **“Developer”**), whose registered office is located at 550 W Merrill St., Suite 100, Birmingham, Michigan 48009, and physical address located at 2060 Coolidge Hwy., Berkley, MI 48072 and the City of Berkley, a Michigan municipal corporation (the **“City”**), whose address is 3338 Coolidge Hwy., Berkley, Michigan 48072 and amends the Planned Unit Development Agreement approved by the City Council on July 22, 2024.

RECITALS:

A. On October 14, 2023, the City of Berkley Planning Commission recommended approval of The Columbia Planned Unit Development (“PUD”).

B. On May 28, 2024, the City of Berkley Planning Commission recommended the approval of a revised version of the PUD with conditions.

C. The PUD was brought before the City Council on June 17, 2024, for review and approval of the PUD.

D. The City Council approved the PUD Plan for the Project and the PUD with conditions on June 17, 2024.

E. The City Council approved this PUD Agreement on July 22, 2024 and it was recorded in the Oakland County Register of Deeds at Liber 59580 Page 151 (“PUD Agreement”)

F. Developer submitted an amended plan of the PUD to the Planning Commission for consideration and voting on March 18, 2025 (“Amended PUD”).

G. On March 18, 2025, the City of Berkley Planning Commission recommended approval of this Amended PUD.

H. This Amended PUD was brought before the City Council on June 2, 2025, for review and approval.

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I. The City Council Approved this Amended PUD Agreement on June 2, 2025.

J. The Amended PUD will be developed in accordance with the PUD Agreement and this Amended PUD Agreement, including the following pertinent information:

PUD Agreement and Amended PUD Agreement: The PUD Agreement remains in full force and effect, except directly and explicitly modified and amended in this Amended PUD Agreement. To the extent that there are any inconsistencies, this Amended PUD shall control and govern.

Location: The legal address of the Property is 2465, 2475, 2466 & 2476 Columbia and 2475 & 2465 Cambridge, Berkley, Michigan 48072.

Legal description: The PUD is established within the property which is legally described on attached **Exhibit A** (the “**Property**”).

Acreage: The PUD is approximately 0.83 acres.

Current Zoning: Gateway District

Proposed Zoning: PUD, Planned Unit Development.

Project: The project includes changing the current zoning classification for the Property from Gateway District to PUD, Planned Unit Development, and development and use of the Property as described and allowed in this the PUD Agreement and this Amended Agreement.

Project Description: The project (“**Project**”) includes one 4-story multifamily development on 0.83 vacant acres located at the east side of Coolidge Highway and off of Cambridge and Columbia roads. The building dimensions are, 210’-2” x 72’-8”. A total of fifty-one (51) apartment units consisting of fifteen (15) studio and thirty-six (36) one-bedroom units. Gross square footage for the building is: ground floor space 3,235 square feet of conditioned space, 11,887 square feet of covered parking; second floor residential 14,203 square feet conditioned space (not including balconies); third floor residential 14,203 square feet of conditioned space (not including balconies); and fourth floor residential 14,203 square feet of conditioned space (not including balconies). Gross square footage for the pocket park fronting Coolidge Hwy. on the south parcel of approximately 5,470 square feet and a carport of approximately 2,250 square feet (there will be two car ports in total totaling approximately 4,500 square feet in car ports). Total gross square feet for the building is 60,075 square feet which includes the carports, exterior wall finishes and inset balconies (not counting wall hung balconies). Total gross square feet for the Project is 45,844 square feet conditioned space, 57,731 square feet counting the covered parking.

Area/Width/Height setback: All front, side, and rear setbacks for Gateway District requirements and proposed Project PUD setbacks. (Sec. 138.526)

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LB (Gateway)	Required	PUD Agreement	Amended PUD Agreement
Height	40' MAXIMUM HEIGHT	40' TO MID RISE	46' FLAT ROOF (6 FOOT DEVIATION REQUIRED) 47'-0" TOP OF PARAPET (50'-0" PARAPET AT FEATURED NW AND SW CORNERS)
Minimum Yard Setback: Front	10 FEET MINIMUM	NORTH BUILDING - 2 FEET, 1/8 INCH. SOUTH BUILDING – 7 FEET, 3 INCHES	DEVIATION TO 0 FEET NO SOUTH BUILDING
Sides	ZERO (0) FEET MINIMUM	NO CHANGE	NO CHANGE
Rear	10 FEET MINIMUM	NO CHANGE	NO CHANGE
Lot Coverage	NO MAXIMUM	90% +/-	80%
Use	SEE SECTION 138-427. – PRINCIPAL USES PERMITTED IN GATEWAY DISTRICT	NO CHANGE TO SECTION 138-427. NO REQUIRED GROUND FLOOR COMMERCIAL.	DEVIATION FOR NO REQUIRED COMMERCIAL USE ON GROUND FLOOR DEVIATION FOR SOUTHERN MOST PARCEL, ALLOW PARKING AND PARK NOT OTHERWISE ALLOWED IN THE GATEWAY DISTRICT; DEVIATION TO ALLOW ACCESSORY STRUCTURE WITH NO PRINCIPAL USE STRUCTURE ALLOW PARKING FOR PRIMARY STRUCTURE TO BE ON SEPARATE SOUTHERN PARCEL AND NOT CONTAINED ON THE SAME PARCEL

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Site Plan: Site Plan prepared by Krieger Klatt Architects, Site Plan No. PPUD-01-25 last revised on April 25, 2025, attached hereto as **Exhibit B**, along with) Giffels Webster Site Plan, revised April 25, 2025, **Exhibit B(1)**, and complete Site Plan Package containing all auxiliary and supplemental documents and information **Exhibit B(2)** and the notes set forth by Hubbell, Roth & Clark, Inc. dated March 5, 2025 **Exhibit B(3)**, and City of Berkley Department of Public Works dated March 3, 2025 **Exhibit B(4)**. The Site Plan satisfies the conditions placed on recommending approval as set forth by the City Planning Commission and approved by the City Council on June 2, 2025.

Public Safety Review: The City of Berkley Public Safety Review, dated February 27, 2025, attached hereto as **Exhibit C**.

Utility Review: Hubbell, Roth & Clark, Inc, dated March 5, 2025, attached hereto as **Exhibit D**.

RCOC: Road Commission for Oakland County (“**RCOC**”) not required as all roads at issue are under City authority and not that of the RCOC.

Traffic Impact Study: The Columbia Traffic Impact Assessment Memo prepared by Mohamed Aguib, PE, dated October 24, 2023, attached as **Exhibit E**.

Parking

Current SITE / Zoning	Project Site Parking	Zoning Relief from Current Ordinance in PUD	Amended PUD
2 PER DWELLING UNIT	1.35 PER DWELLING UNIT	.65 PER DWELLING UNIT (114 required; 77 provided)	.65 PER DWELLING UNIT (102 required; 69 provided)
30 PERCENT CAP ON COMPACT PARKING SPACES	36 PERCENT	6 PERCENT VARIANCE ABOVE ZONING REQUIREMENT	5 PERCENT VARIANCE ABOVE ZONING REQUIREMENT

The Site Plan of the Amended PUD illustrates that between designated site parking spots, and street parking there is sufficient parking. The calculations used on the attached site plan include the use of the bicycle parking credit provided for in Section 138.268 of the Berkley Zoning Ordinance to achieve 1.35 spaces per unit. The number of available parking is sufficient to service the needs of the residents of the Columbia. The Site Plan also shall conform to the conditions approved by the City Planning Commission and adopted by City Council in its approval related to the number of compact car spots allowed. The Project shall include signage directing all visitors and delivery vehicles to park on Coolidge. Additionally, the lease agreements for units in the Project

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WJ Ventures, LLC and City of Berkley

shall contain a requirement that all tenants direct their guests and delivery drivers to park on Coolidge.

Location of Parking Lots

The Parking Lots for the Project will be located on the 1st floor of the building and in covered parking in the carport on the southern parcel (south side of Columbia).

Sidewalks

The Westerly sidewalks of the building will be enhanced to include extensive landscaping along the Coolidge Highway front enhancing the streetscape and softening the building, a park providing additional landscaping and softening of the Project will be installed on the southern parcel of the Project.

Landscaping

The Project will be developed in accordance with the landscape plan prepared by Giffels Webster Engineers, dated April 25, 2025, which satisfies the conditions placed on recommending approval as set forth by the City Planning Commission and the landscape plan is attached hereto as **Exhibit F (“Landscape Plan”)**, which identifies the landscaping to be installed within the Project. Changes in the species and locations of plantings must be approved administratively by the City Planner if the City Planner determines them to be consistent with the spirit of the Landscape Plan.

Remainder Unchanged.

Traffic

The Project is expected to generate the traffic impacts as indicated in the Traffic Impact Study. The results of additional site generated traffic are expected to be within roadway and intersection capacities. In comparison to a code compliant site plan (e.g., what the Developer could build without a PUD or any variance), the proposed development is reducing the number of added vehicles to the roadway network. See **Exhibit E**, Columbia Traffic Impact Assessment Memo prepared by Mohamed Aguib, PE, dated October 24, 2023.

Columbia Storm/ Rainwater Improvements

Unchanged, with the clarification that a reduced size detention system will be used due to the Project as amended having less impervious area coverage by building and parking lots.

PUD Agreement/Phasing

Unchanged.

Architectural Elevations

The building elevations for the Project which are attached as **Exhibit G** to this Agreement have been approved by the City and shall be deemed to be in compliance with the Commercial Architectural Standards located within Section 138.679 of the Berkley Zoning Ordinance and commercial development standards.

Exterior Lighting

Exterior lighting for the Project shall be in accordance with Photometric Plan and Exterior Light Details included as part of the Site Plan (**Exhibit B2**, p.11).

Signage

Unchanged

Recognizable Benefit & Satisfaction of Standards for Approval.

This Agreement, including the Site Plan, comprise a PUD that provides a recognizable benefit as required by Division 18, Section 138-533 of Berkley's Zoning Ordinance. Moreover, the Project meets the standards for approval to proceed as a PUD as set forth in Section 138-537 of Berkley's Zoning Ordinance. Both the Berkley Planning Commission and the Berkley City Council have found and affirmatively voted that the Project meets the ordinance requirements to proceed as a PUD and to enter into this Agreement.

The Project provides for the protection and integrity of the surrounding neighborhood and provides access to multifamily living options in accordance with the Master Plan and needs of the community. The Berkley Planning Commission specifically found that the following recognizable benefits were provided by the Project under Section 138-533(2) extensive landscaping; (4) provision of open spaces; and (5) efficient consolidation of poorly dimensioned parcels. The Berkley City Council specifically found that the following recognizable benefits were provided by the Project under Section 138-533(2) extensive landscaping; (4) provision of open spaces; and (5) efficient consolidation of poorly dimensioned parcels.

PUD Allowable Uses

Unchanged.

The City desires to ensure that the Property is developed and used in accordance with the approved Site Plan and all applicable laws and regulations.

NOW, THEREFORE, the Developer and the City have, in consideration of the mutual covenants, promises and conditions described in this Amended Agreement, and with the expressed understanding that this Amended Agreement contains important and essential terms as part of the final approval of the Amended PUD, agree as follows.

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WJ Ventures, LLC and City of Berkley

1. Incorporation of Recitals. The parties acknowledge and represent that the foregoing recitals are true and accurate and binding upon the parties. The recitals and the exhibits are incorporated into this Amended Agreement.

2. Amended PUD Approval.

(A) The parties acknowledge and agree that the development of the Project has been granted final Amended PUD approval under the City's Ordinances, subject to the terms and conditions of this Agreement and has also received final site plan approval as under the Berkley Zoning Ordinance (the "Zoning Ordinance"). Approval of the PUD, PUD Agreement, Amended PUD, includes approval of the Site Plan and this Amended Agreement, and all Exhibits attached to the Agreement or this Amended Agreement, which take precedence over and supersede any and all prior reports, agreements, plans and other submissions to and/or approvals by the City relative to the Project.

(B) The Developer's right to develop the Property as provided for in this Amended Agreement and the Exhibits attached to this Agreement are fully vested upon the City's execution of this Amended Agreement and the recording of this Amended Agreement at the Oakland County Register of Deeds.

3. Zoning. Unchanged.

4. Development. Unchanged.

5. Zoning Ordinance Requirements. Unchanged.

6. Minor Modifications. The Project may be developed over a period of time during which there may arise a need to make minor modifications to the Site Plan. During the Term of this Agreement (as defined in Section 7). The minor modifications fall into two categories: (A) modifications that comply with the City's Zoning Ordinance in effect at the time the of the modification ("Ordinance Compliant Modifications") and (B) modifications that are not directly governed by the City's Zoning Ordinance in effect at the time ("Ordinance Neutral Modifications").

(A) Ordinance Compliant Modifications: The City hereby delegates to City staff the duty to review and approve the following Ordinance Compliant Modifications to the Site Plan that may be requested by Developer: (a) modification in the number or type of units in the Project so long as the change complies with the City's Zoning Ordinances in effect at the time of the modification; (b) modification to parking per unit ratios so long as the modification complies with the City's Zoning Ordinance in effect at the time of the modification; and (c) any modification that Developer chooses to make that is compliant with the City's ordinances in effect at the time of the modification and during the Term as defined in Paragraph 7. For the avoidance of doubt, the City staff has been delegated the power, right, and duty to approve Ordinance Compliant Modifications as set forth above without approval by the Planning Commission or City Council and shall be done so without notice or public hearing.

The Columbia – Amended PUD Agreement

WJ Ventures, LLC and City of Berkley

(B) Ordinance Neutral Modifications: The City hereby delegates to City staff the right to review and approve the following Ordinance Neutral Modifications to the Site Plan that may be requested by Developer: (a) relocations of any building such that the relocation is in the general vicinity of the footprint of such building as shown on the Site Plan; (b) internal rearrangement of parking areas, roads, sidewalks, storm drainage facilities and other utilities, such that the area and density requirements and the overall character of the Site Plan are not materially affected; (c) addition of any refuse, dumpster, or recycling receptacle; (d) changes in materials or color based on availability of products so long as the replacement material is of equal quality and character; and (d) change in the location or species of required trees and other landscape material. For the avoidance of doubt, the City staff has been delegated the power and has the right to approve Ordinance Neutral Modifications as set forth above without approval by the Planning Commission or City Council and may be done without notice or public hearing but is not required to do so.

Any other modification sought by Developer that exceeds the authority delegated to City staff in this Section shall be submitted to the Planning Commission for review and approval, with Developer having the right to initially appeal the Planning Commission's determination to City Council. Ordinance Compliant Modifications and approved Ordinance Neutral Modifications shall not require an amendment to this Agreement, but shall become a part of this Agreement as if fully stated herein.

7. Site Plan Validity. The Property is currently fully developed as multifamily duplexes. Notwithstanding anything to the contrary contained in this Agreement or the City ordinances, this Amended Agreement, the Amended PUD approval, Site Plan approval and Developer's right to develop the Property in accordance with the terms of this Amended Agreement shall remain in effect for a period of 6 years beginning upon the approval of the Amended PUD Agreement (the "Term") provided that building drawings and plans are submitted for approval and permitting within thirty (30) months following the approval of the Amended PUD Agreement. If the plans are not submitted for approval and permitting within thirty (30) months following the approval of the Amended PUD Agreement, Developer may request an extension of time which shall be considered through good faith and reasoned deliberation by the City Council. The Term will commence on the date of executing this Amended Agreement. During the Term the City shall not require annual extensions of the Site Plan approval and Amended PUD approval. At the expiration of the Term, further extensions of Site Plan or Amended PUD approval period(s) may be granted by the City Council, which shall not be unreasonably withheld by the City. If the Term expires without extension and construction is not completed, the Amended PUD approval will be considered expired.

8. Building Permits. Unchanged.

9. Consents/Approvals. Unchanged.

10. Sale of Property. Unchanged.

11. Zoning Board of Appeals. Unchanged.

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WJ Ventures, LLC and City of Berkley

12. Integration/Amendments. Unchanged.
13. Severability. Unchanged.
14. Governing Law. Unchanged.
15. Waiver. No waiver of any breach of this Amended Agreement shall be held to be a waiver of any other or subsequent breach.
16. Remedies. Unchanged.
17. Inconsistency; Effect of Approval; Ordinances in Effect. Unchanged
18. Authority. The signers of this Agreement warrant and represent that they have the authority to sign this Agreement on behalf of their respective principals and the authority to bind each party to this Agreement according to its terms. Further, each of the parties represents that the execution of this Agreement has been duly authorized and is binding on such party.
19. Limitation of City's Liability. Unchanged
20. Binding Effect. Unchanged
21. Curb Cut. Unchanged
22. Utilities, Sanitary Sewer, Storm Sewer and Water. Unchanged
23. Storm Water Management and Drainage. Unchanged
24. Certificate of Occupancy. Unchanged.
25. Maintenance Agreement. Unchanged
26. Permits. Unchanged.
27. Observation of Site Improvements. Unchanged
28. Soil Erosion Controls. Unchanged
29. Access to Property. Unchanged
30. Performance and Completion Assurances. Unchanged.
31. Financial Incentives. Unchanged.

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WJ Ventures, LLC and City of Berkley

32. Cooperative Exchange of Information. Unchanged.

33. Counterparts. Unchanged.

34. Force Majeure Delay. If there is any delay in the performance by Developer of any obligation which is caused by fire, flood, earthquake or other acts of God, adverse weather conditions, strike, tariffs, lockout, acts of public enemy, riot, insurrection, terrorism, changes in governmental interpretations, governmental delays, new governmental regulations or governmental regulation of or general delays in the sale or transportation of materials, supplies or labor, epidemic or pandemic, including, but not limited delays caused by shelter-in-place orders or other governmental rule, regulation or action that causes businesses or governmental agencies or departments to shut down or delay in the performance of their respective obligations, or any other matter beyond Developer's reasonable control, the applicable time period shall be extended for a period of time equal to such delay. In the case of the epidemic/pandemic, such extension shall be equal to the period of time equal to such delay, plus six (6) months.

35. Staging Area. Unchanged.

This Agreement has been executed on the later of the signature dates set forth below.

[Signatures and notarization are contained on the following page]

The City of Berkley,
a Michigan Municipal Corporation

WJR Ventures, LLC
a Michigan limited liability company

Date: _____

The foregoing Planned Unit Development Agreement was acknowledged before me this ____ day of _____, 2025, by Wayne M. Wudyka, the Managing Member of WJ Ventures, LLC, a Michigan limited liability company, on behalf of said company

Drafted by:

Ryan C. Plecha, Esq.
Counsel for WJ Ventures, LLC
&
P. Daniel Christ, Esq.
Counsel for the City of Berkley

When Recorded Return to:

Kristen Kapelanski, AICP
Community Development Director
City of Berkley
3338 Coolidge Hwy.
Berkley, MI 48072

THE REGULAR MEETING OF THE BERKLEY CITY PLANNING COMMISSION WAS CALLED TO ORDER AT 7:00 PM, MARCH 18, 2025 AT BERKLEY CITY HALL BY CHAIR LISA KEMPNER.

The minutes from this meeting are in summary form capturing the actions taken on each agenda item. To view the meeting discussions in their entirety, this meeting is broadcasted on the city's government access channel, WBRK, every day at 9AM and 9PM. The video can also be seen, on-demand, on the city's YouTube channel: <https://www.youtube.com/user/cityofberkley>

PRESENT: Josh Stapp
Shiloh Dahlin
Lisa Kempner
Eric Arnsman

ABSENT: Lisa Hamameh, Joe Bartus, Mike Woods

ALSO PRESENT: Kristen Kapelanski, Community Development Director
Greg Patterson, City Council Liaison

Motion by Commissioner Dahlin to excuse the absences of Commissioners Hamameh, Bartus and Woods.
Motion supported by Commissioner Stapp.

Voice Vote to excuse the absences of Commissioners Hamameh, Bartus and Woods.

AYES: 4

NAYS: 0

ABSENT: Hamameh, Bartus, Woods

MOTION CARRIED

* * * * *

APPROVAL OF AGENDA

Motion by Commissioner Arnsman to approve the agenda and supported by Commissioner Stapp.

Voice vote to approve the agenda

AYES: 4

NAYS: 0

ABSENT: Hamameh, Bartus, Woods

MOTION CARRIED

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APPROVAL OF THE MINUTES

Motion by Commissioner Dahlin to approve the minutes of the January 28, 2025 regular Planning Commission meeting and supported by Commissioner Arnsman.

Voice vote to approve the meeting minutes of January 28, 2025.

AYES: 4

NAYS: 0

ABSENT: Hamameh, Bartus, Woods

MOTION CARRIED

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COMMUNICATIONS

NONE

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CITIZEN COMMENTS

NONE

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OLD BUSINESS

NONE

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NEW BUSINESS

1. **Amendment to Planned Unit Development PPUD-01-25: The Columbia:** The applicant, Krieger Klatt Architects on behalf of WJ Ventures LLC, 2465 Columbia, 2475 Columbia, 2468 Columbia and 2476 Columbia and 2475 Cambridge, is requesting an amendment to the Planned Unit Development previously approved for a multiple-family development on the east side of Coolidge Highway, south of Cambridge Road and north and south of Columbia Road

Community Development Director Kapelanski presented the project noting that the applicant is requesting an amendment to a previously approved Planned Unit Development. The Planning Commission should consider only the changes proposed and whether they affect or change the previously found public benefits. Changes to the plan include the following:

- The southern building has been eliminated and all units have been consolidated into one building on the northern-most parcel. The pocket park originally on the eastern half of the southern parcel has been moved to the west, fronting Coolidge. Carports are proposed as accessory structures on the southern parcel.
- Conversion of the mansard roof into a flat deck roof. Height per the Zoning Ordinance is measured to the mid-point of a mansard roof and the top of a flat roof. In the original PUD, the top of the mansard roof was labeled as 45 ft. with a midpoint of 40 ft., the maximum allowed height. Since a flat top roof is measured differently, with a proposed height of 46 ft., this will require a deviation.
- The first-floor lobby has been increased in size.
- The unit count has decreased from 57 to 51 units.
- Balcony locations have been added and adjusted.
- The façade has been updated to have similar materials but with a different color palate. Cement board siding has been added as an accent material on the east, north and south elevations. Siding can be permitted by the Planning Commission if the Commission finds that the standards in Section 138-678 have been met.
- The parking lot layout has been revised and the number of parking spaces has been reduced from 77 to 69. With the reduction of units, this equals a ratio of 1.35 spaces per unit. The current approved plan also proposed 1.35 spaces per unit. The parking lot reconfigurations result in a more usable parking area without the use of tandem spaces.
- Landscaping throughout the site has been adjusted to accommodate the proposed layout changes.
- The refuse area has been moved under the podium of the building.

The applicant's team, WJ Ventures, LLC, Giffels Webster and Krieger Klatt Architects went through a presentation visually showing the above changes.

Chair Kempner opened the public hearing at 7:20PM. No one wished to speak and no correspondence was received. The public hearing was closed at 7:20PM.

The Planning Commission asked and the applicant agreed to work with staff to better activate the proposed park area.

The Commission went through the three previously found public benefits and agreed those were still met. They also agreed the standards of approval in Section 138-537 have been met.

Motion by Commissioner Stapp and supported by Commissioner Arnsman to recommend approval of the proposed amendment to PUD-01-25 The Columbia with the following ordinance deviations:

- Deficient use (Non-residential is not provided on the first floor.)
- Deficient use (The southernmost parcel contains a public park space/plaza and parking for the proposed multifamily building. Parking and a park are not permitted uses in the Gateway Zoning District.)
- Deficient front setback - west (10 ft. required, 0.5 ft. provided)
- Deficient building height (40 ft. required, 46 ft. provided)
- Deficient number of parking spaces (102 required, 69 provided)
- Deficient number of compact car spaces (30% permitted, 35% proposed)
- Deficient use (Accessory structure – carports- located on a parcel without a principal use.)
- Deficient parking location (Off-street parking facilities must be located on the same parcel as the principal use. Parking proposed on southern parcel.); and

With the following conditions:

- The applicant work with staff to better activate the proposed park space; and

With the following findings:

- The minimum of three of the seven public benefits listed in Section 138-533 have been met including:
 - That the project provides extensive landscaping, beyond the site plan requirements;
 - That the project provides open space or a public plaza; and
 - That the project provides efficient consolidation of poorly dimensioned parcels; and
- The standards for approval in Section 138-537 are met.

Roll call vote on the motion to recommend approval of PUD-01-25 The Columbia.

AYES: 4

NAYS: 0

ABSENT: Hamameh, Bartus, Woods

2. Planning Commission Liaison for Chamber of Commerce

The Planning Commission elected to postpone discussion of this item until more members were present.

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LIAISON REPORT

Commissioners provided updates on the Downtown Development Authority and Zoning Board of Appeals.

City Council Member Patterson provided an update on recent Council decisions.

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COMMISSIONER COMMENTS

NONE

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STAFF COMMENTS

NONE

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ADJOURNMENT

Motion to adjourn by Commissioner Dahlin supported by Commissioner Arnsman.

Voice vote for adjournment

AYES: 4

NAYS: 0

ABSENT: Hamameh, Bartus, Woods

With no further business, the meeting was adjourned at 7:52 p.m.

2465 Cambridge PPUD-01-23

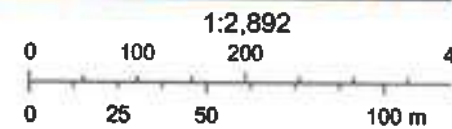


Thu Sep 07 2023

☐ Override 1

Override 1

RCOCAppMapService_5220



OC Parcels Plus 2465 Cambridge

Search

Value Shape Buffer Report Spatial Result

Zoom All

Clear:

selected: 1



2517353002

SITEADDRESS: 2465 CAMBRIDGE RD



2465 Cambridge Rd, Berkeley, MN

Score: 90.9

[Zoom to](#)

40ft



OC Parcels Plus 2465 Cambridge

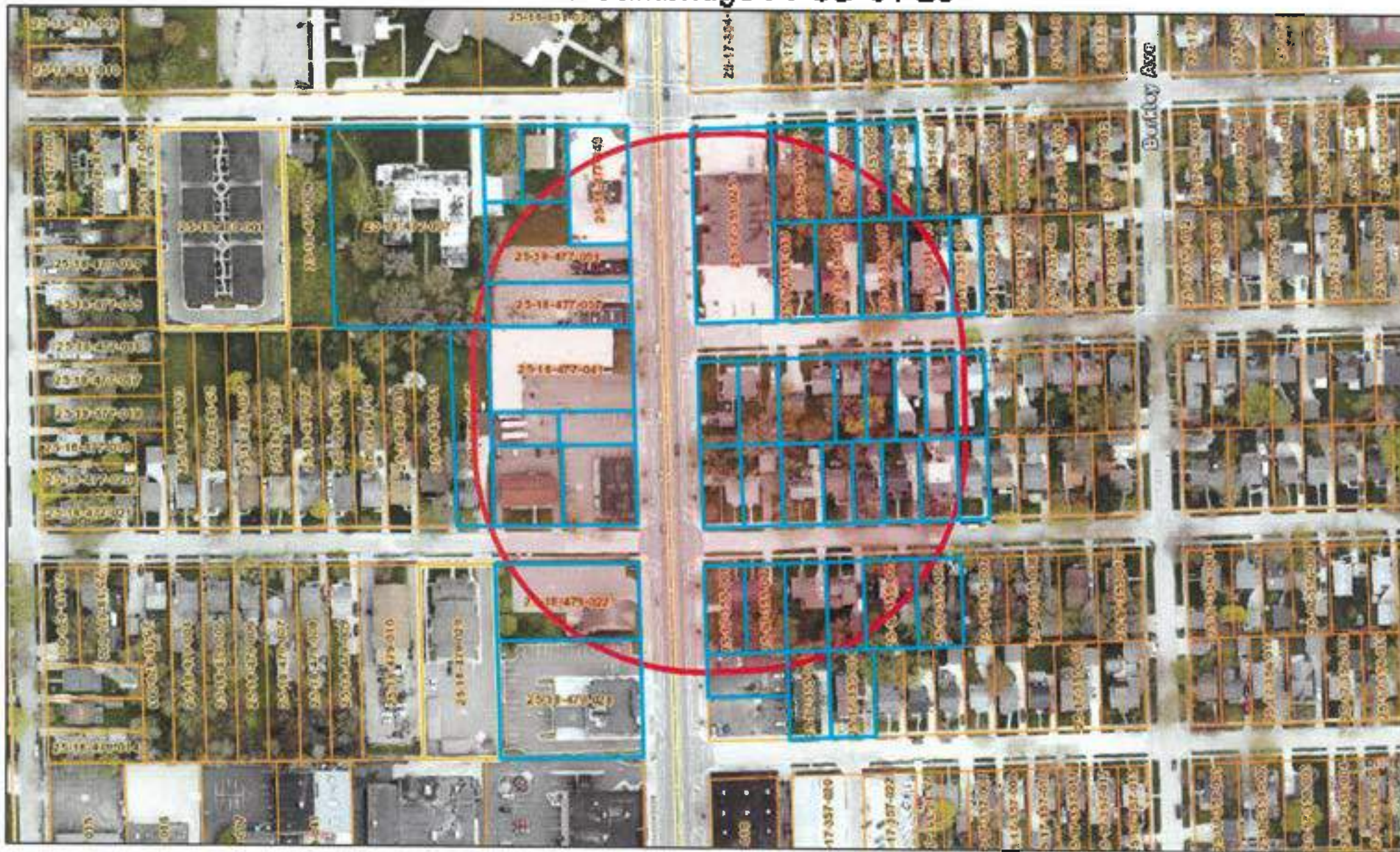


83.183 42.491 Degrees

200ft



2475 Cambridge PPUD-01-23

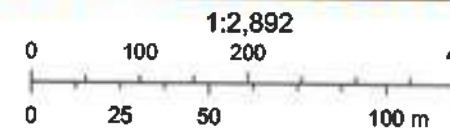


Thu Sep 07 2023

Override 1

Override 1

RCOCAppMapService_5220



OC Parcels Plus 2475 Cambridge

Search

Value Shape Buffer Report Spatial Result

Zoom All

Clear:

selected: 1



2517353001

SITEADDRESS: 2475 CAMBRIDGE RD

40ft



117.00



Cambridge Rd

50

53.40

54.12

117.00

125

56.30

25-17-353-001
100-353-41-52

125

55.00

55.00

25-17-353

25-17-353

101-30

2475 Cambridge Rd, Berkley, MI

Score: 90.9

[Zoom to](#)

OC Parcels Plus 2475 Cambridge

-83.185 42.491 Degrees

200ft



2465 Columbia PPUD-01-23

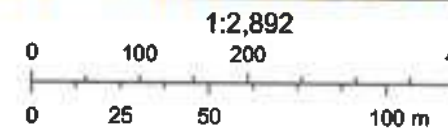


Thu Sep 07 2023

 Override 1

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 RCOCAppMapService_5220



OC Parcels Plus 2465 Columbia

Search

- X

Value Shape Buffer Report Spatial Result

Zoom All

Clear:

selected: 1

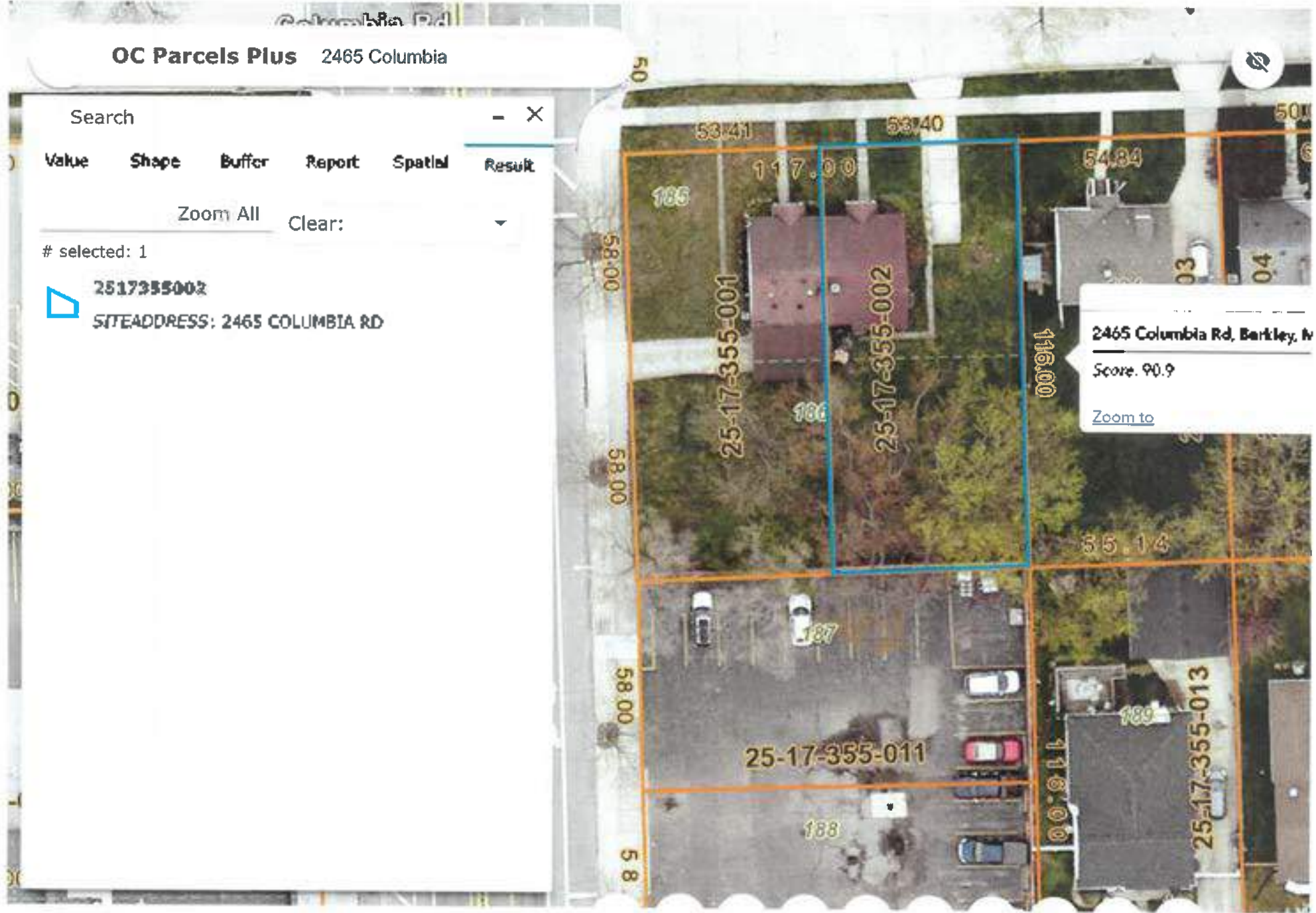


2517355002

SITEADDRESS: 2465 COLUMBIA RD



40ft



2465 Columbia Rd, Barkley, W

Score: 90.9

[Zoom to](#)

OC Parcels Plus 2465 Columbia

-03.182 42.490 Degrees

200ft



2468 Columbia PPUD-01-23

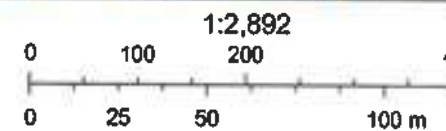


Thu Sep 07 2023

 Override 1

 Override 1

 RCOAppMapService_5220





2468 Columbia Rd, City of Berkeley, 48072

Score: 90.9

[Zoom to](#)

-83 183 42.491 Degrees

40ft



OC Parcels Plus

2468 Columbia

-83.184 42.491 Degrees

200ft



This aerial map displays a residential neighborhood with property boundaries and parcel numbers. A red circle highlights a specific area in the center-left, and a blue circle highlights a specific area in the center-right. The map shows a grid of streets and numerous small residential lots, each with a unique parcel number.

☐ Override 1

RCOAppMapService_5220

1:2,892

OC Parcels Plus 2475 Columbia



2475 Columbia Rd, Berkley, MI, 48072

Score: 90.9

[Zoom to](#)

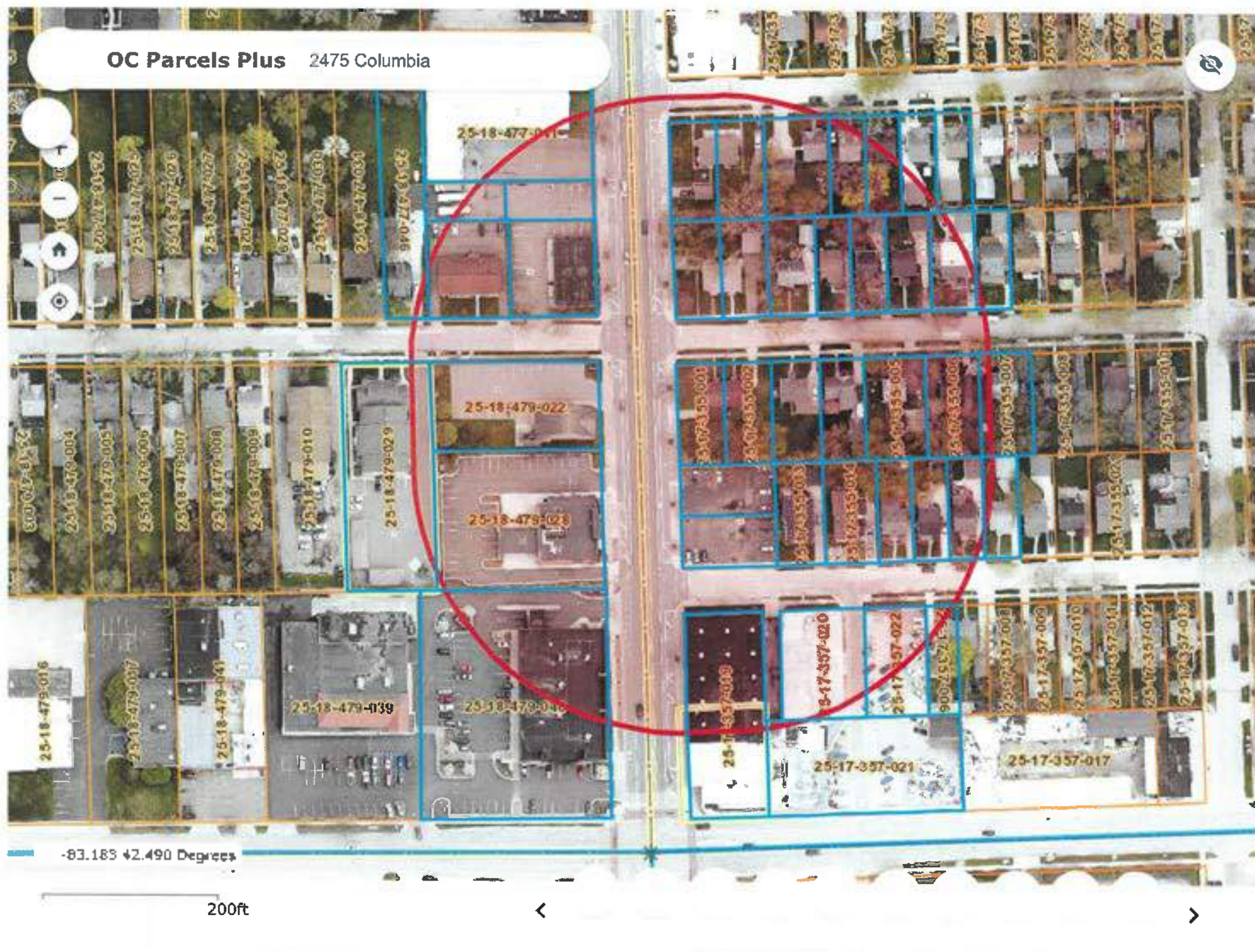
18-479-028

-83.183 42.490 Degrees

40ft



OC Parcels Plus 2475 Columbia



2476 Columbia PPUD-01-23

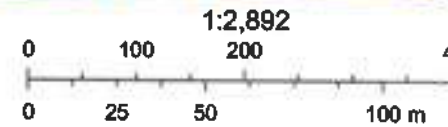


Thu Sep 07 2023

 Override 1

 Override 1

 RCOAppMapService_5220



25-18-477-048

25-18-477-040

Columbia Rd

200.00

-83.183 42.491 Degrees

40ft

55.00

55.00

55.30

25-17-353-015

53.37

117.00

53.40

53.41

117.00

185

2476 Columbia Rd, Berkley, NJ, 48072

Score: 90.9

[Zoom to](#)

54.41

25-17

54.41

54.34

117

OC Parcels Plus 2476 Columbia



200ft



THE CITY OF BERKLEY
Community Development Department
3338 Coolidge Highway, Berkley, Michigan 48072
(248) 658-3320

NOTICE OF PUBLIC HEARING
BERKLEY CITY PLANNING COMMISSION

NOTICE IS HEREBY GIVEN, that in accordance with the City of Berkley Code of Ordinances, Chapter 138 *Zoning*, Article VI *Administration and Enforcement, Amendments*, a Public Hearing will be held by the Berkley City Planning Commission on Tuesday, March 18 at 7:00 p.m. or as near thereto as the matter may be reached at the City Hall Council Chambers, 3338 Coolidge Highway, Berkley, Michigan.

Application Number PPUD-01-25

The applicant, Krieger Klatt Architects on behalf of WJ Ventures LLC, 2465 Columbia, 2475 Columbia, 2468 Columbia and 2476 Columbia and 2475 Cambridge and 2465 Cambridge, Parcel #'s 25-17-353-001, 25-17-353-002, 25-17-355-002, 25-17-355-001, 25-17-353-016 and 25-17-353-015 is requesting an amendment to the Planned Unit Development previously approved for a multi-family development.

Complete application information is available for review at www.berkleymi.gov/community-development/development-projects.

Comments regarding the proposed site plan may be made in person on the night of the Public Hearing or may be made in writing. All written comments must be submitted in person to the Community Development Department or sent via email to planning@berkleymi.gov before 5:00 PM on the date of the Planning Commission meeting.

You can watch the meeting: [youtube.com/cityofberkley](https://www.youtube.com/cityofberkley)

KRISTEN KAPELANSKI
COMMUNITY DEVELOPMENT DIRECTOR

THE CITY OF BERKLEY
Community Development Department
3338 Coolidge Highway, Berkley, Michigan 48072
(248) 658-3320

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KRISTEN KAPELANSKI
COMMUNITY DEVELOPMENT DIRECTOR

Publish Once:

Royal Oak Tribune
Royal Oak, Michigan
Friday, February 28, 2025



CITY OF BERKLEY

COMMUNITY DEVELOPMENT

3338 COOLIDGE HWY, BERKLEY, MICHIGAN 48072

MEMORANDUM

To: Krieger Klatt Architects, Applicant

From: Kristen Kapelanski, Community Development Director

Subject: **PPUD-01-25 The Columbia PUD Amendment – Review #1**

Date: February 27, 2025

Project History

The project includes 2465, 2475, 2466 and 2476 Columbia and 2475 and 2465 Cambridge. The current zoning of the site is PUD. The City Council approved a Planned Unit Development Plan for the subject property in 2024, which included a four-story multi-family development on 0.83 acres at the aforementioned addresses. Fifty-seven units were contained in two buildings with associated parking, utilities and landscaping. The Council approved the request with the following ordinance deviations and recognizing the following public benefits. (All PUD projects must meet at least three of the seven public benefits identified in Section 138-533.) In addition, the Council found that the standards for approval in Section 138-537 had been met.

Ordinance Deviations

- Deficient setback along Coolidge (10 ft. required, 2 ft. 1/8 in. provided for north building, 7 ft. 3in. provided for south building)
- Deficient number of parking spaces (114 required, 77 provided)
- Deviation to allow for an overage on the total number of compact car spaces (30% permitted, 36% provided)
- Deficient use (non-residential is not provided on the first floor)

Public Benefits

- The project provides extensive landscaping, beyond site plan requirements.
- The project provides open space or a public plaza.
- The project provides efficient consolidation of poorly dimensioned parcels.

Proposed Amendments to the Planned Unit Development

The applicant has now proposed amendments to the approved PUD. All amendments outside of those identified as being eligible for administrative approval in the approved PUD Agreement must go back through the standard PUD approval process. In this case, the applicant is also proposing to modify the proposed deviations as well.

The proposed amendments are detailed in the applicant's provided narrative and are summarized below. Since this request is for an amendment to an existing, approved PUD, the Planning Commission and City Council should focus their review on those elements of the plan that have changed.

Proposed Amendments

- The southern building has been eliminated and all units have been consolidated into one building on the northern-most parcel. The pocket park originally on the eastern half of the southern parcel has been moved to the west, fronting Coolidge. Carports are proposed as accessory structures on the southern parcel.
- Conversion of the mansard roof into a flat deck roof. Height per the Zoning Ordinance is measured to the mid-point of a mansard roof and the top of a flat roof. In the original PUD, the top of the mansard roof was labeled as 45 ft. with a midpoint of 40 ft., the maximum allowed height. Since a flat top roof is measured differently, with a proposed height of 46 ft., this will require a deviation. Parapets are also proposed for visual interest and screening. Those are not included in the overall height measurements. See the applicant's February 14, 2025 narrative for additional information.
- The first-floor lobby has been increased in size.
- The unit count has decreased from 57 to 51 units.
- Balcony locations have been added and adjusted.
- The façade has been updated to have similar materials but with a different color palate. Cement board siding has been added as an accent material on the east, north and south elevations. Siding can be permitted by the Planning Commission if the Commission finds that the standards in Section 138-678 have been met.
- The parking lot layout has been revised and the number of parking spaces has been reduced from 77 to 69. With the reduction of units, this equals a ratio of 1.35 spaces per unit. The current approved plan also proposed 1.35 spaces per unit. The parking lot reconfigurations result in a more usable parking area without the use of tandem spaces.
- Landscaping throughout the site has been adjusted to accommodate the proposed layout changes.
- The refuse area has been moved under the podium of the building.

Proposed Deviations from the Zoning Ordinance

Deviations that are proposed as part of the PUD plan are listed below. New deviations are bolded. Revised deviations are italicized.

- Deficient use (Non-residential is not provided on the first floor.)
- ***Deficient use (The southernmost parcel contains a public park space/plaza and parking for the proposed multifamily building. Parking and a park are not permitted uses in the Gateway Zoning District.)***
- *Deficient front setback -west (10 ft. required, 0.5 ft. provided)*
- ***Deficient building height (40 ft. required, 46 ft. provided)***

- *Deficient number of parking spaces (102 required, 69 provided)*
- *Deficient number of compact car spaces (30% permitted, 35% proposed)*
- ***Deficient use (Accessory structure – carports- located on a parcel without a principal use.)***
- ***Deficient parking location (Off-street parking facilities must be located on the same parcel as the principal use. Parking proposed on southern parcel.)***

Items to be Addressed

Bolded items must be addressed prior to the Planning Commission meeting. The remaining items can be conditions of approval should the Planning Commission or City Council make motions of approval.

- Three barrier free spaces are required. One additional barrier free space must be added to the north parking lot.
- A screen wall is indicated along the east property line. A detail matching the height of the screen wall included in the current approved plan and indicating masonry to match the building should be included in the plan set.
- **A photometric plan complying with ordinance standards must be provided prior to the Planning Commission meeting.**
- **The square footage of the previously approved public park/plaza along with the square footage of the currently proposed public park/plaza should be provided.**
- **Material samples should be brought to the Planning Commission meeting.**
- All mechanical rooftop units will need to be screened with metal panels if screening is not achieved via parapets.

PUD Recognizable Benefits

Section 138-533 of the Zoning Ordinance indicated that a PUD application shall demonstrate to the City that the PUD protects the integrity of the surrounding neighborhood and meets at least three of the following site design elements that could not be attained through a project designed under conventional zoning.

- High-quality architectural design, beyond the site plan requirements of this title.
 - There is visual interest created by the differing materials, inset balconies and screening panels on the first floor. The Planning Commission and City Council did not previously identify this as a recognized benefit but may want to reevaluate based on the revised elevation.
- Extensive landscaping, beyond the site plan requirements of this title.
 - The revised plan includes landscaping beyond what has been seen in recent projects, consistent with the prior approval. Additionally, and also consistent with the prior approval, the applicant will plant six new trees in the Coolidge right-of-way and provide landscaping for the properties to the east, should they be willing to accept it. The Planning Commission and City previously found that this was a recognized benefit.
- Preservation or enhancement of historic resources.
 - This does not apply and was not previously identified as a recognized benefit.

- Provision of open space and public plazas.
 - The previous plan includes streetscape improvements along Coolidge, a pocket park and associated furnishings on the southern parcel. All of these except the streetscape improvements have been included in the proposed plan. Additionally, the pocket park is more open and accessible to the public in its revised location. The Planning Commission and City Council previously found this was a recognized benefit. The applicant should consider adding the previously included streetscape elements back into the plan.
- Efficient consolidation of poorly dimensioned parcels.
 - This element has not changed. The plan includes the consolidation of three adjacent parcels. The Planning Commission and City Council did not find this as a recognizable benefit as part of the previous approval.
- Shared vehicular access between properties or uses.
 - The proposed plan changes do not offer new vehicle access between properties or uses. The Planning Commission and City Council did not previously include this in the list of recognizable benefits.
- A complementary mix of uses or a variety of housing types.
 - The conversion of three two-family homes to a multiple- family building offers a better transition from Coolidge's commercial corridor to the adjacent neighborhood. No proposed uses on the site have been changed from the previous approval. The Planning Commission and City Council did not recognize this as a benefit in the previous approval.

PUD Standards for Approval

As set forth in Section 138-537 of the Zoning Ordinance, the City shall consider the following standards when determining whether to approve, approve with conditions or deny a proposed PUD.

- A recognizable and material benefit to the ultimate users of the project and to the community, where such benefit would otherwise be unfeasible or unlikely to be achieved without application of the PUD regulations.
 - This standard was previously found to be met by the Planning Commission and City Council. As identified as part of the previously approved PUD, the Master Plan identifies the need to diversify housing choices in the corridor. These apartments offer a housing option for small households in various stages of life. The barrier-free accessibility of the units reflects a housing style that is needed in the community based on current housing availability and population forecasts. The changes proposed as part of the PUD amendment do not change the use of the property in terms of a recognizable benefit as identified here.
- The proposed type and density of use shall not result in an unreasonable increase in the need for or burden upon public services, facilities, streets and utilities.
 - In terms of parking, the applicant is proposing the same parking standard that was previously approved. The Department of Public Works and the City Engineer have not identified any deficiencies when compared to the previous plan. The proposed amendment does not alter the previous finding that this standard has been met.

- There shall be underground installation of utilities, including electricity and telephone, unless otherwise approved by the City.
 - The Department of Public Works and the City Engineer have not identified any deficiencies when compared to the previous plan. The proposed amendment does not alter the previous finding that this standard has been met.
- The proposed development shall be consistent with the public health, safety and welfare of the City.
 - This standard has been met, as the project was previously, and continues to be consistent with the housing and development goals of the Master Plan and Downtown Master Plan. The proposed amendment does not alter the previous finding that this standard has been met.
- The proposed development shall not result in an unreasonable negative economic impact on surrounding properties.
 - The Planning Commission and City Council previously found that this standard was met with the inclusion of the screen wall adjacent to the neighboring residential properties and landscaping offered to the adjacent lots. Those items are included in this amendment as well.
- The proposed development shall be consistent with the goals and policies of the Master Plan.
 - This standard was previously considered met through with a proposal that makes convenient use of a shallow property within the Coolidge commercial corridor. The intention of the Master Plan, particularly within this area, is to offer diverse housing options and improve the aesthetic character. The proposed amendments do not alter this conclusion.

Staff Recommendation

The applicant should address the following items prior to the Planning Commission meeting:

- **A photometric plan complying with ordinance standards must be provided prior to the Planning Commission meeting.**
- **The square footage of the previously approved public park/plaza along with the square footage of the currently proposed public park/plaza should be provided.**

Staff recommends approval of the proposed PUD amendment as it is consistent with the previous findings of public benefit and Section 138-537 of the Zoning Ordinance.



CITY OF BERKLEY

DEPARTMENT OF PUBLIC WORKS

3238 BACON AVE, BERKLEY, MICHIGAN 48072

Transmittal Memo

To: Kristen Kapelanski, Community Development Director (via email)

Cc: Kim Anderson, Community Development Department (via email)
Adam Wozniak, Public Works Superintendent (via email)

From: Shawn Young, Deputy City Manager for Public Services

Date: March 3rd, 2024

Subject: "The Columbia" Review #3

We have reviewed the site plans provided by the Community Development Department on February 14th, 2025. Please find our comments below:

1. The site plans will be reviewed by the City's engineering consultant, Hubbell, Roth & Clark (HRC). Among other things, HRC will review the storm water calculations and grading.
2. The existing 6" water main on Columbia from the 12" watermain in Coolidge to just past the planned construction limits shall be replaced with new 8' water main to support the fire flow needs of the structure and limit the potential for future disruptions to the building. This work will require an EGLE Permit.
3. More information such as a geotechnical report and water table levels are needed before approval of the proposed stormwater detentions system is approved.
4. A traffic control plan shall be added to the plan set to account for any proposed Right of Way impacts. This includes any potential sidewalk closures during construction.

I recommend approval pending these items.

Feel free to call with any questions or concerns. Thank you.



March 5, 2025

City of Berkley
3338 Coolidge Highway
Berkley, Michigan 48072

Attn: Ms. Kim Anderson, Zoning Administrator

Re: The Columbia
2465-2476 Columbia & 2465-2475 Cambridge
PUD Application Site Plan – Review No. 4
City of Berkley, Michigan

HRC Job No. 20230156.02

Dear Ms. Anderson:

In accordance with your request, as Consulting Engineers for the City of Berkley, our office has conducted a Site Plan review for the above referenced proposed development for compliance with the City of Berkley's engineering and site plan requirements. The plan was prepared by Giffels Webster of Birmingham, Michigan with a revision date of February 14, 2025. The scope of the project includes the demolition of three existing single-family residential buildings, garages, and all other accessory site features and the construction of a new 4-story, 51-unit apartment complex. Per the City's standards and requirements for construction, we hereby offer the following comments:

General:

1. The number of parking spaces shown does not meet the ordinance requirements of 2 per residential unit. We will defer to Community Development to discuss the possibility of an exception to the parking requirements. **This item is still applicable.**

Public & Private Utilities:

2. The existing water and sewer leads are to be abandoned and new leads will be constructed. The sanitary leads must be abandoned via a spot liner or another DPW approved method, within 2 feet of the main. Consult with the DPW prior to abandonment of existing leads. **This note has been added to the plans. This item has been satisfactorily addressed.**
3. The proposed sewer leads must include a new connection (TwisTee sewer saddle, as per the City DPW, or approved equal). The Applicant is responsible for any roadwork and restoration associated with the new utility connections which must be shown on the plans. The DPW must be contacted prior to the commencement of work within the right-of-way (ROW). **This note has been added to the plans. This item has been satisfactorily addressed.**

Storm Drainage and Detention/Sanitary Sewer:

4. The Grading Plan shows that stormwater will be contained within the site and directed towards the proposed underground storage system. **The site grading appears satisfactory.**

5. Stormwater runoff calculations are provided for a 100-year storm event in accordance with Oakland County standards. The proposed underground storage system volume exceeds the required detention volume according to these calculations and the provided details of the storage system.
This item is satisfactory.
6. A restricted outlet structure is proposed along the outlet pipe from the underground storage system to the city sewer main. The minimum restrictor size is 1 inch as per OCWRC, if the weir outlet design is to be used.
7. Note that downspouts are not permitted to discharge directly into the combined sewer system and must be directed onto grass, landscape or other green space area. They may also be directed through the proposed restricted-outlet structure. The location of the proposed downspouts and the above note must be shown on the plans.
8. Further details must be provided for full engineering approval, including all pavement cross sections and utility connections. The pavement cross section details must include a detail showing that the depth of cover above the underground storage system matches the manufacturer requirements.
This item is still applicable.
9. A traffic control plan will be required prior to full engineering site plan approval. **This item is still applicable.**
10. A profile view of all utilities, including how and what is connected to the underground stormwater storage area, must be included on the plans.
11. Details of the connections to the Aquaswirl structure must be shown on the plans.
12. A geotechnical investigation is recommended to evaluate the feasibility of the proposed stormwater management system. The report and soil logs should be submitted along with the plans, including the elevation of the groundwater table, soil types encountered, and the design recommendations or comments on the proposed stormwater collection system.
13. The developer will be required to prepare and enter into a perpetuity maintenance agreement with the City for the proposed private stormwater systems. The Applicant must contact the City DPW regarding this item prior to commencing construction.

Recommendation:

Based on our aforementioned comments, we recommend conditional approval of the proposed Site Plan, pending the submittal of the above-mentioned detail sheets and revisions.

If you have any questions or require any additional information, please contact our office.

Very truly yours,

HUBBELL, ROTH & CLARK, INC.



Edward D. Zmich
Civil Department Manager

EDZ/kab

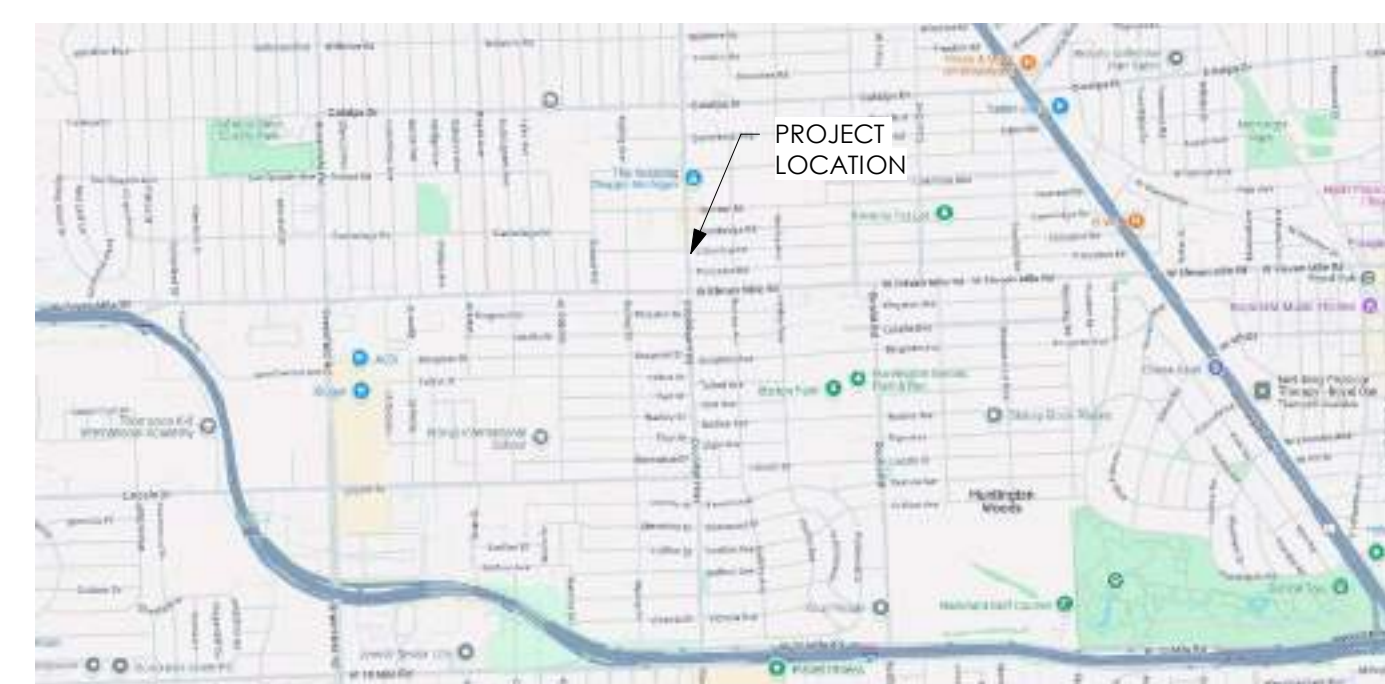
pc: City of Berkley; Shawn Young, Kristen Kapelanski
HRC; R. Alix, K. Barauskas, File

2476 Columbia Rd. Berkley, MI 48072

WJ Ventures, LLC
2060 Coolidge Hwy.
Berkley, MI 48072
P.248.246.7878

Krieger | Klatt Architects Inc.
400 E. Lincoln Ave.
Royal Oak, MI 48067
P.248.414.9270.
F.248.414.9275

Giffels Webster
1025 E. Maple, Suite 100
Birmingham, MI 48009
P.248.852.3100



Civil Sheet Index		
Sheet No.	Title	02.14.2025 Revised PUD 03.13.2025 Revised PUD Rev #1
C-200	Topographic & Boundary Survey	•
C-210	Schedule of Structures	•
C-300	Demolition Plan	•
C-600	Grading and Paving Plan	•
C-700	Utility Plan	•
C-720	Utility Calculations	•
LS-100	Landscape Plan	•
1 of 1	Photometric Plans & Details	•

Architectural Sheet Index		
Sheet No	Title	Revised PUD 02.14.2025 03.13.2025 Revised PUD Rev #1
G.001	Cover Sheet	• •
G.002	Aerial Renderings	• •
G.003	Perspective Renderings	• •
A.100	First Floor Plan	• •
A.101	Typical Floor Plan (Second - Third)	• •
A.102	Fourth Floor Plan	• •
A.103	Roof Plan	• •
A.200	Elevations	• •
A.201	Rendered Elevations	• •
A.202	Carport Elevations	• •
A.400	Building Sections	• •

400 E. Lincoln Ave. | Royal Oak, MI 48067
P: 248.414.9270 F: 248.414.9275
www.kriegerklatt.com

WJ Ventures, LLC
2060 Coolidge Hwy.
Berkley, MI
48072

The Columbia
2476 Columbia Rd.
Berkley, MI 48072

[illegible]

A circular professional seal for the State of Michigan. The outer ring contains the text "STATE OF MICHIGAN" at the top and "LICENSED ARCHITECT" at the bottom, separated by stars. The center of the seal contains the name "JASON P. KRIEGER", the title "ARCHITECT", and the license number "No. 53578". A large, stylized signature is written across the seal, overlapping the text and the outer ring.

Do not scale drawings. Use
calculated dimensions only.
Verify existing conditions in
field.

PRELIMINARY NOT FOR CONSTRUCTION

Cover Sheet

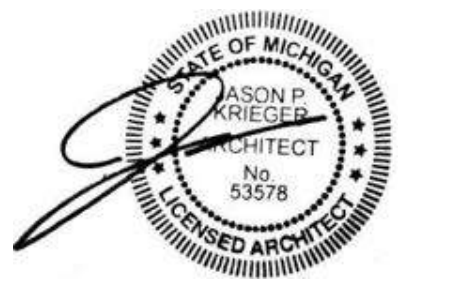
Project Number

Sheet Number:

G.001

[illegible]

Seal: _____



Note:

Do not scale drawings. Use calculated dimensions only. Verify existing conditions in field.

North Arrow: _____

Sheet Title:
Aerial Renderings

Project Number: _____

Project Number

Scale: _____

Sheet Number: _____

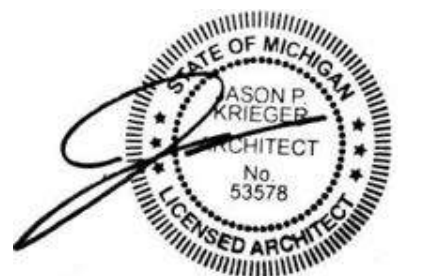
G.002



PRELIMINARY NOT FOR CONSTRUCTION

[illegible]

Seal: _____



Note:

Do not scale drawings. Use
calculated dimensions only.
Verify existing conditions in
field.

North Arrow:

Sheet Title:

Perspective
Renderings

Project Number: _____

Project Number

Scale: _____

Sheet Number: G.003

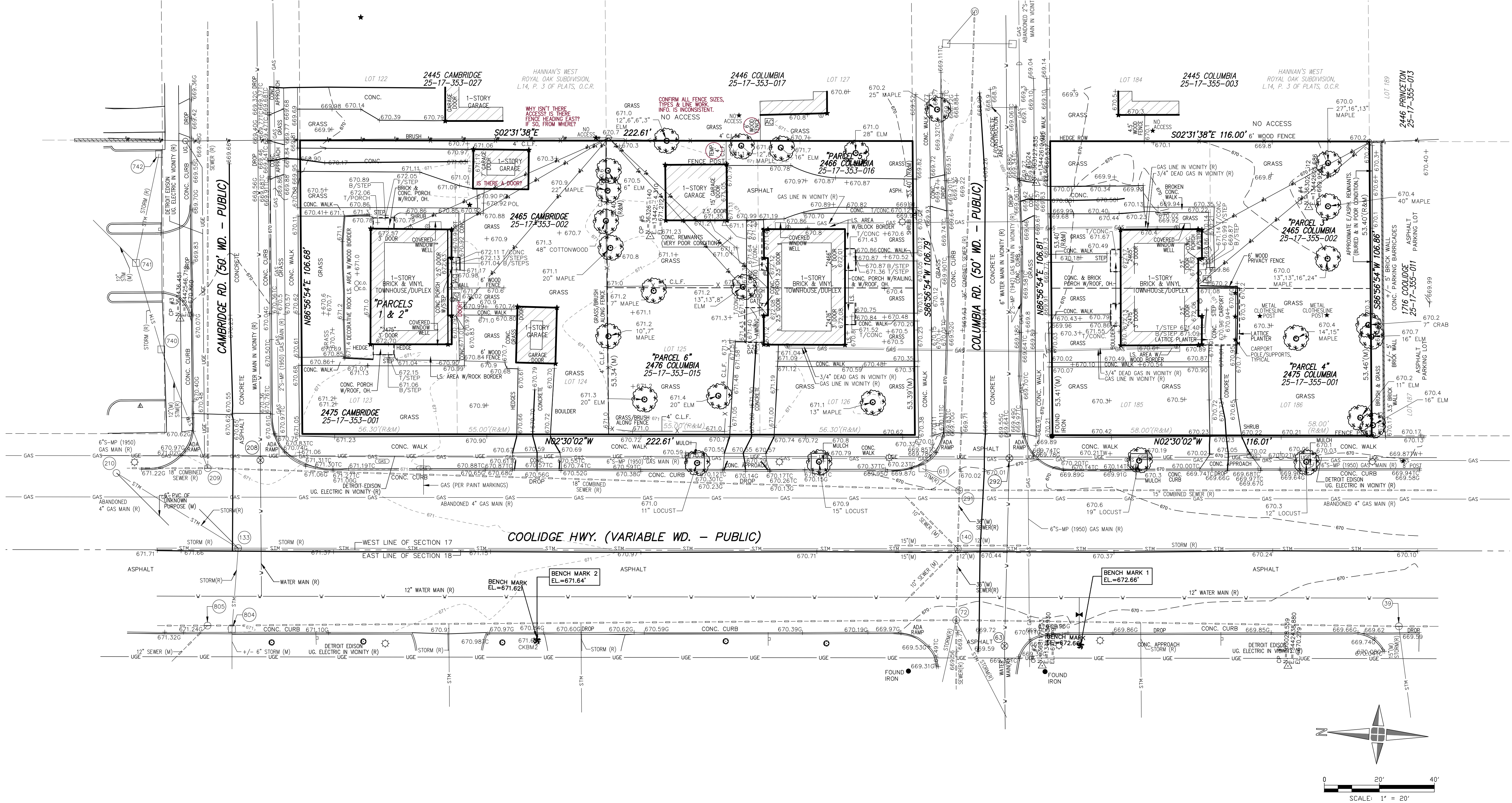


PRELIMINARY NOT FOR CONSTRUCTION

LEGEND – EXISTING

SECTION LINE	⑤ SAN. MH	⑥ GAS METER	★ MISC. TOPO. FEATURE
E/ PAVEMENT CURB	○ C.O. SAN. CLEAN OUT	⑦ GAS RISER	▲ SURVEY CONTROL POINT
E/ GRAVEL	△ SAN. RISER	⑧ GAS VALVE	● FOUND IRON
CL ROAD	⊙ SAN. PUMP STATION	⑨ GAS MH	● FOUND NAIL
E/ WALK	⊙ COMB. MH	⑩ TELE. RISER	✕ F. CUT CROSS
RAILROAD	⊙ GATE VALVE	⑪ TELE. MH	⊙ SECTION COR.
E/ BRICK	⊙ HYDRANT	⑫ TELE. CROSS BOX	⊙ FENCE POST
MISC. LINE	⊙ WATER VALVE	⑬ CABLE RISER	★ BENCHMARK
FENCE	⊙ WATER METER	⑭ POST INDICATOR VALVE	● FOUND PIPE
GUARD RAIL	⊙ WELL HEAD	⑮ FDC CONNECTION	● FOUND MON.
WALL	⊙ IRRIGATION CONTROL BOX	⑯ STORM MH	ASPH. ASPHALT
BLDG. LINE	⊙ OVERHANG LINE	⑰ T/ BANK	CONC. CONCRETE
OH	⊙ RAILING	⑱ B/ BANK	A.C. AIR CONDITIONER
OVERHEAD WIRES	⊙ CATCH BASIN	⊙ CL DITCH/STREAM	G.P. GUARD POST
RAILING	⊙ SHRUB LINE	⊙ TREE LINE	C.L.F. CHAIN-LINK FENCE
OVERHANG LINE	⊙ WATER EDGE	⊙ WETLAND LINE	D.L. DOOR LEDGE
T/ BANK	⊙ STORM LINE	⊙ SAN. LINE	F.F. FINISHED FLOOR
B/ BANK	⊙ SAN. LINE	⊙ WATER LINE	O.H. OVERHANG
CL DITCH/STREAM	⊙ GAS	⊙ UGE	F.I. FOUND IRON
SHRUB LINE	⊙ UGE	⊙ UGT	S.I. SET IRON
TREE LINE	⊙ CTV	⊙ PAINT STRIPE	F.I.P. FOUND IRON PIPE
WATER EDGE	⊙ PLD LINE	⊙ STEAM	(M) MEASURED
WETLAND LINE	⊙ STEAM	⊙ TEL.	(R) RECORD
STORM LINE	⊙ TEL.	⊙ CABLE TV LINE	F.M. FOUND MONUMENT
SAN. LINE	⊙ CABLE TV LINE	⊙ COMBINED SEWER	S.N. SET NAIL
WATER LINE	⊙ COMBINED SEWER	⊙ PLD LINE	CMP CORRUGATED METAL PIPE
GAS	⊙ PLD LINE	⊙ STEAM	RCP REINFORCED CONCRETE PIPE
UGE	⊙ STEAM	⊙ TEL.	GLD GROUND LIGHT
UGT	⊙ TEL.	⊙ CABLE TV LINE	LS. LANDSCAPING
CTV	⊙ CABLE TV LINE	⊙ COMBINED SEWER	(A) ENCROACHMENT
PAINT STRIPE	⊙ COMBINED SEWER	⊙ PLD LINE	
COMBINED SEWER	⊙ PLD LINE	⊙ STEAM	
PLD LINE	⊙ STEAM	⊙ TEL.	
STEAM	⊙ TEL.		
TEL.			

MISS DIG DESIGN TICKET NUMBER:
B012982740-00B



UTILITY STATEMENT

THE UNDERGROUND UTILITIES SHOWN HAVE BEEN LOCATED FROM FIELD SURVEY INFORMATION AND EXISTING DRAWINGS. THE SURVEYOR MAKES NO GUARANTEES THAT THE UNDERGROUND UTILITIES SHOWN COMPRISE ALL SUCH UTILITIES IN THE AREA, EITHER IN SERVICE OR ABANDONED. THE SURVEYOR FURTHER DOES NOT WARRANT THAT THE UNDERGROUND UTILITIES SHOWN ARE IN THE EXACT LOCATION INDICATED ALTHOUGH HE DOES CERTIFY THAT THEY ARE LOCATED AS ACCURATELY AS POSSIBLE FROM INFORMATION AVAILABLE.

(R) = UTILITY SHOWN FROM RECORDS OR PLANS, & FIELD LOCATED WHERE POSSIBLE.

PRIOR TO THE PLANNED BUILDING IMPROVEMENTS, AND/OR CONSTRUCTION, THE RESPECTIVE UTILITY COMPANIES MUST BE NOTIFIED TO STAKE THE PRECISE LOCATION OF THEIR UTILITIES.

BENCH MARK DATA

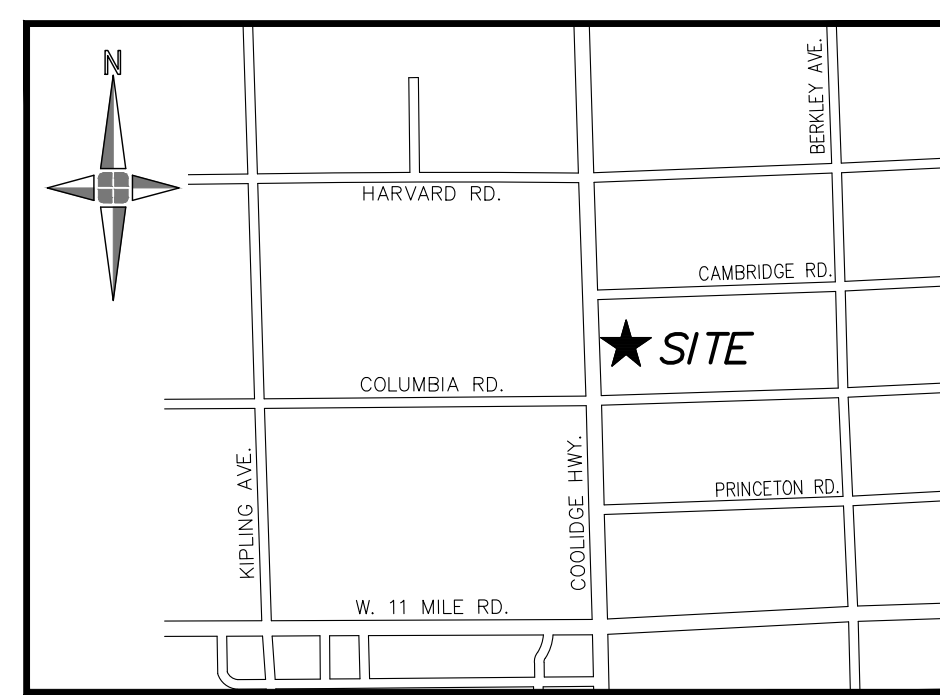
(NAVD83) DATUM WHICH DATUM? NOT LISTED ON BENCH LOOP

BENCH MARK 1
ARROW ON HYDRANT (1996), LOCATED ON THE WEST SIDE OF COOLIDGE HIGHWAY, +/- 3.5 FEET OFF OF THE BACK OF CURB, AND +/- 36 FEET SOUTH OF COLUMBIA ROAD.
ELEVATION=672.66'
(SHOWN GRAPHICALLY)

BENCH MARK 2
SE BOLT IN A LIGHT POLE BASE, LOCATED ON THE WEST SIDE OF COOLIDGE HIGHWAY, +/- 3 FEET OFF OF THE BACK OF CURB, AND +/- 150 FEET NORTH OF COLUMBIA ROAD.
ELEVATION=671.64'
(SHOWN GRAPHICALLY)

NOTES:

- SURVEY IN ACCORDANCE WITH TITLE COMMITMENT NO. 21223250-LP, ISSUED BY BELL TITLE AGENCY, DATED AUGUST 17, 2021
- TELEPHONE, CABLE TV, STEAM, AND PUBLIC LIGHTING UTILITY MAPS WERE NOT AVAILABLE AT TIME OF SURVEY.
- THE FOLLOWING WERE UTILIZED FOR THE REPRESENTATION OF CONDUIT, IN THIS DRAWING:
 - CONSUMERS ENERGY RECORD DRAWINGS: GAS LEAD LOCATION SHOULD BE CONSIDERED AS APPROXIMATE. CONTACT MISS DIG TO LOCATE ALL CONDUIT, PRIOR TO CONSTRUCTION.
 - THE CITY OF BERKLEY WATER, SEWER, AND STORM RECORD DRAWING: DUE TO THE SCHEMATIC NATURE OF THIS RECORD, PRECISE LOCATION OF CONDUIT CANNOT BE DETERMINED. CONDUIT LOCATION, IN THIS DRAWING, SHOULD BE CONSIDERED AS APPROXIMATE. CONTACT MISS DIG TO LOCATE ALL CONDUIT, PRIOR TO ANY CONSTRUCTION.
 - OLDER DETROIT EDISON RECORD DRAWINGS: AS THESE RECORD DRAWINGS ARE NOT CURRENT, CONDUIT MAY EXIST, ALTHOUGH NOT DEPICTED. IN ADDITION, DUE TO THE SCHEMATIC NATURE OF THESE RECORDS, PRECISE LOCATION CANNOT BE DETERMINED. CONDUIT LOCATION SHOULD BE CONSIDERED AS APPROXIMATE, WHERE NOT FIELD LOCATED. CONTACT MISS DIG TO LOCATE ALL CONDUIT, PRIOR TO ANY CONSTRUCTION.
 - OLDER LEAD 3/CENTURY LINK DRAWING: CONDUIT IS SHOWN, IN THIS RECORD, AS BEING OUTSIDE OF THE SCOPE OF THIS DRAWING. HOWEVER, AS THIS RECORD DRAWING IS NOT CURRENT, CONDUIT MAY EXIST, ALTHOUGH NOT DEPICTED. CONTACT MISS DIG TO LOCATE ALL CONDUIT, PRIOR TO ANY CONSTRUCTION.
- MEASURED BEARINGS ARE BASED ON THE STATE PLANE COORDINATE SYSTEM, MICHIGAN SOUTH (2113), NAD83.
- SEE SHEET 2 FOR STRUCTURE LIST.



LOCATION MAP
(NOT TO SCALE)

giffels webster

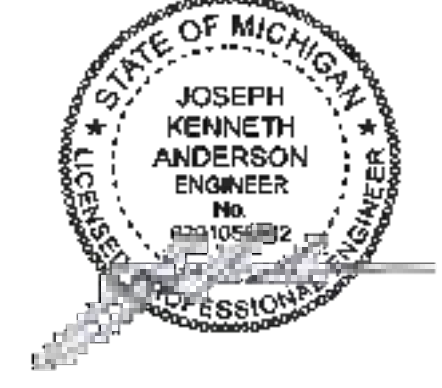
Engineers
Surveyors
Planners
Landscape Architects

1025 East Maple Road
Suite 100
Birmingham, MI 48009

p (248) 852-3100
f (313) 962-5068
www.giffelswebster.com

Executive:	AW
Manager:	JKA
Designer:	JKA
Quality Control:	AW
Section:	25
	T-03-N R-10-E

Professional Seal:



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DATE:	ISSUE:
07.21.2023	Submit PUD to City
08.25.2023	Submit #2 PUD to City
08.28.2023	Submit #3 PUD to City
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04.28.2024	Revised PUD
06.04.2024	Rev. per PC Conditions for Approval
02.14.2025	Revised Plans for PUD Amendment
04.25.2025	City Council Submittal

Developed For:

WJ VENTURES, LLC

2060 COOLIDGE HWY
BERKLEY, MI 48072

248-246-7878

TOPOGRAPHIC &
BOUNDARY SURVEY
THE COLUMBIA

CITY OF BERKLEY
OAKLAND COUNTY
MICHIGAN

Date:	07.21.2023
Scale:	1" = 20'
Sheet:	C-200
Project:	20388.00

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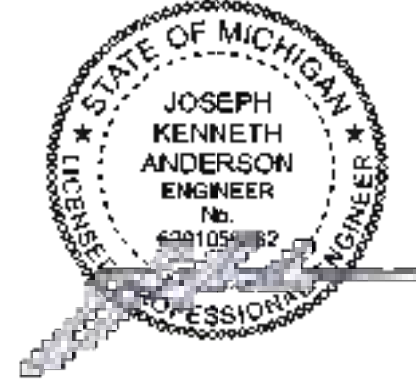


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Developed For:

WJ VENTURES, LLC

2060 COOLIDGE HWY
BERKLEY, MI 48072

248-246-7878

SCHEDULE OF
STRUCTURES

THE COLUMBIA

CITY OF BERKLEY
OAKLAND COUNTY
MICHIGAN

Date:	07.21.2023
Scale:	N/A
Sheet:	C-210
Project:	20388.00

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SCHEDULE OF STRUCTURES

STRUCTURE	TYPE	SIZE OF PIPE	RIM	DROP	INVERT	DIRECTION	COMMENTS
39	STORM MANHOLE	CL. 15" CHANNEL	669.49	-12.23	657.26	NORTH & SOUTH. FLOWS NORTH.	
		15"	669.49	-7.14	662.35	WEST	
63	GATE VALVE	T/VALVE	669.60	-3.48	666.12		
		18"	669.89	-13.50	656.39	NORTH	
72	COMBINED MANHOLE	CL. 36" CHANNEL	669.89	-14.10	655.79	EAST & WEST. FLOWS EAST.	
		18"	669.89	-13.52	656.37	SOUTH	
133	STORM MANHOLE	CL. 15" CHANNEL	671.51	-6.20	665.31	NORTH & SOUTH. FLOWS SOUTH.	
		10"	671.51	-5.05	666.46	NORTHEAST	
140	SANITARY MANHOLE	10"	671.51	-4.90	666.61	WEST	
			670.46	-5.00	665.46	NORTHWEST	
		15"	670.46	-6.80	663.66	NORTH	
			670.46	-12.55	657.91	NORTH	
		10"	670.46	-5.30	665.16	NORTHEAST	
		CL. 36" CHANNEL	670.46	-14.90	655.56	EAST & WEST. FLOWS EAST.	
		12"	670.46	-6.60	663.86	SOUTH	
			670.46	-12.20	658.26	SOUTH	
208	GATE VALVE	T/VALVE	670.84	-4.05	666.79		
209	STORM MANHOLE	18"	671.01	-12.85	658.16	NORTH	
		12"	671.01	-9.60	661.41	EAST-NORTHEAST	
			671.01	-12.25	658.76	EAST	
210	UNKNOWN MANHOLE	6" PVC	671.01	-13.00	658.01	SOUTH. FLOWS SOUTH.	
			671.03	-3.77	667.26	SOUTHWEST	INVERT PLUS OR MINUS. HEAVILY OFFSET. NO OTHER PIPES VISIBLE. FULL OF WATER.
291	SANITARY MANHOLE	18"	670.10	-13.85	656.25	NORTH	
		12" PVC	670.10	-3.85	666.25	NORTHEAST	
		CL. 36" CHANNEL	670.10	-14.80	655.30	EAST & WEST	
		18"	670.10	-13.40	656.70	SOUTH	
292	GATE VALVE	T/WATER	669.74	-1.10	668.64		
		T/VALVE	669.74	-3.98	665.76		INVERT PLUS OR MINUS. UNABLE TO DEFINITELY DETERMINE T/VALVE.
611	CATCH BASIN	12"	669.84	-3.32	666.52	SOUTHWEST	NO OTHER PIPES VISIBLE. FULL OF WATER.
740	CATCH BASIN	12"	670.33	-8.00	662.33	EAST-NORTHEAST	NO OTHER PIPES VISIBLE
		12"	670.31	-6.67	663.64	NORTHEAST	NO OTHER PIPES VISIBLE
741	CATCH BASIN	10"	670.31	-6.27	664.04	EAST	NO OTHER PIPES VISIBLE
		12"	670.31	-7.87	662.44	WEST-SOUTHWEST	NO OTHER PIPES VISIBLE
742	CATCH BASIN	10"	669.37	-3.78	665.59	WEST	24" DIAMETER STRUCTURE
804	CATCH BASIN	10"	670.97	-3.90	667.07	EAST. FLOWS EAST.	
		+/- 6"	670.97	-3.02	667.95	WEST	
805	STORM MANHOLE	12"	671.14	-12.30	658.84	NORTHWEST	
		18" CHANNEL	671.14	-13.18	657.96	NORTH & SOUTH. FLOWS SOUTH.	

PROPERTY DESCRIPTION

(PER TITLE COMMITMENT NO. 21223250-LP, ISSUED BY BELL TITLE AGENCY, DATED AUGUST 17, 2021)
LAND IN THE CITY OF BERKLEY, COUNTY OF OAKLAND, MICHIGAN TO WIT:

PARCELS 1 AND 2:

LOTS 123 AND 124, EXCEPT THAT PORTION NOW INCLUDED IN COOLIDGE HIGHWAY, HANNAN'S WEST ROYAL OAK SUBDIVISION, ACCORDING TO THE PLAT THEREOF AS RECORDED IN LIBER 14 OF PLATS, PAGE 3, OAKLAND COUNTY RECORDS.
(TAX ID.: 25-17-353-001 - 2475 CAMBRIDGE)
(TAX ID.: 25-17-353-002 - 2465 CAMBRIDGE)

PARCEL 3:

EAST 53.4 FEET OF LOTS 185 AND 186, HANNAN'S WEST ROYAL OAK SUBDIVISION, AS RECORDED IN LIBER 14, PAGE 3 OF PLATS, OAKLAND COUNTY RECORDS
(TAX ID.: 25-17-355-002 - 2465 COLUMBIA)

PARCEL 4:

LOTS 185 AND 186, EXCEPT EAST 53.40 FEET OF EACH, ALSO EXCEPT WEST 10.19 FEET OF LOT 185 AND WEST 10.17 FEET OF LOT 186, HANNAN'S WEST ROYAL OAK SUBDIVISION, AS RECORDED IN LIBER 14, PAGE 3 OF PLATS, OAKLAND COUNTY RECORDS.
(TAX ID.: 25-17-355-001 - 2475 COLUMBIA)

PARCEL 5:

EAST 53.4 FEET OF LOTS 125 AND 126, HANNAN'S WEST ROYAL OAK SUBDIVISION, AS RECORDED IN LIBER 14, PAGE 3 OF PLATS, OAKLAND COUNTY RECORDS.
(TAX ID.: 25-17-353-016 - 2466 COLUMBIA)

PARCEL 6:

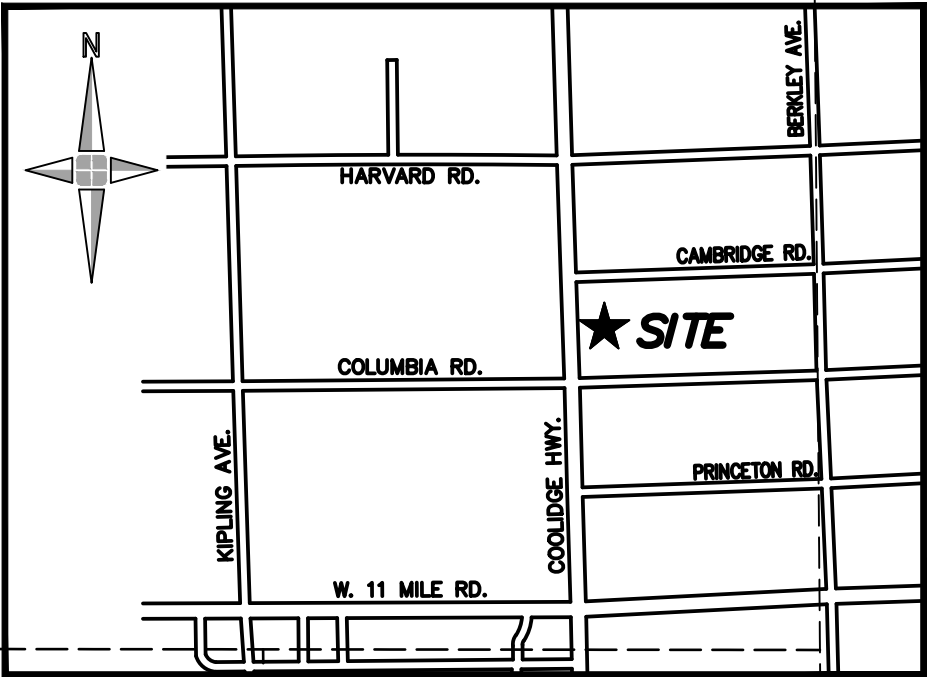
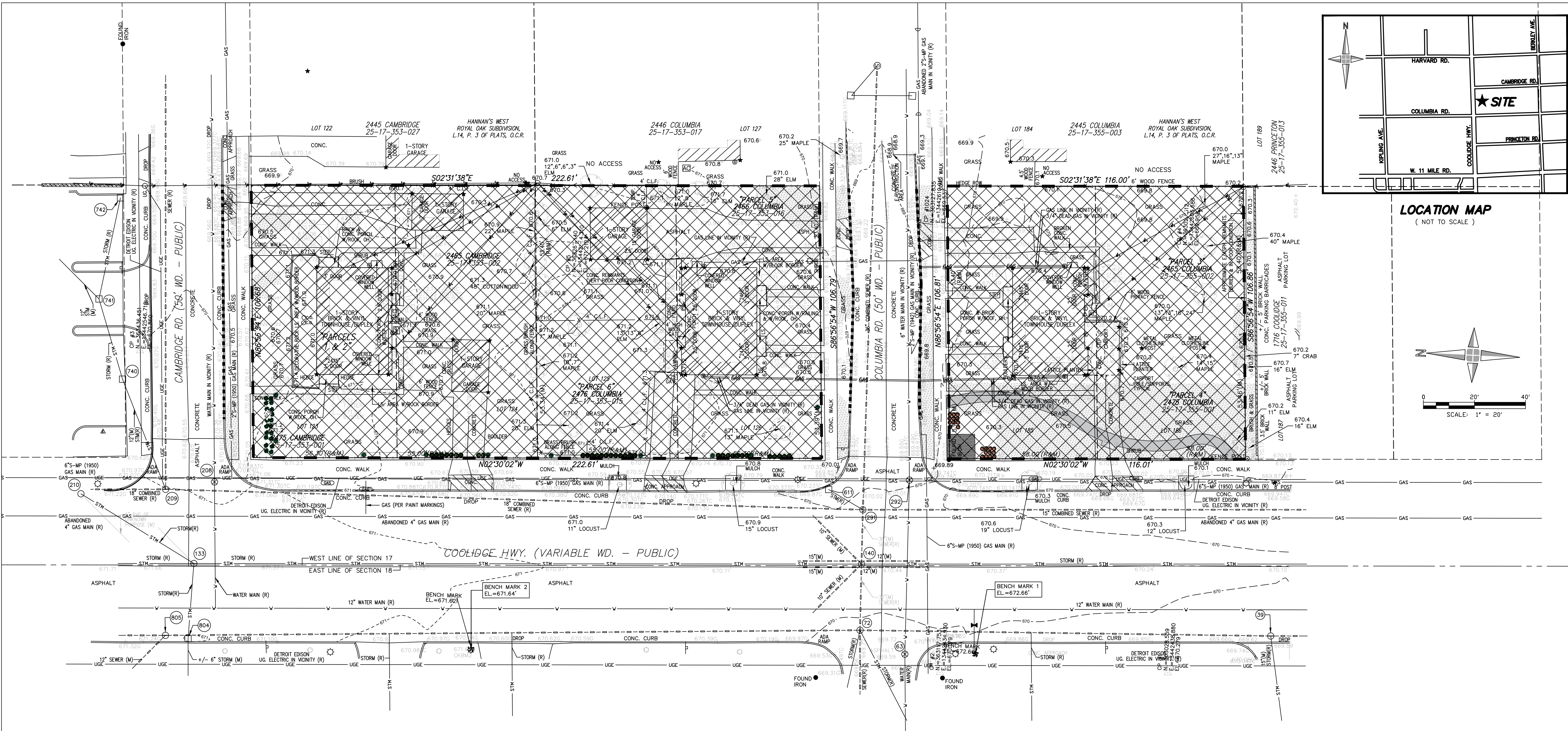
LOTS 125 AND 126, EXCEPT THE EAST 53.40 FEET OF EACH, ALSO EXCEPT WEST 10.25 FEET OF LOT 125, ALSO EXCEPT WEST 10.23 FEET OF LOT 126, HANNAN'S WEST ROYAL OAK SUBDIVISION, AS RECORDED IN LIBER 14, PAGE 3 OF PLATS, OAKLAND COUNTY RECORDS.
(TAX ID.: 25-17-353-015 - 2476 COLUMBIA)

SCHEDULE B – II. EXCEPTIONS

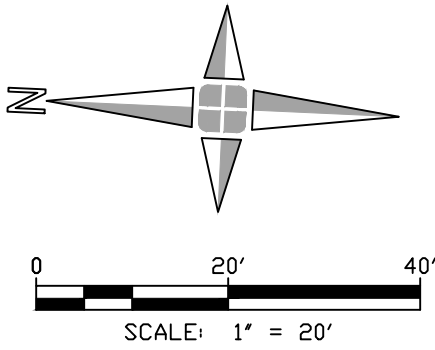
(PER TITLE COMMITMENT NO. 21223250-LP, ISSUED BY BELL TITLE AGENCY, DATED AUGUST 17, 2021)

NO SCHEDULE B – II. EXCEPTIONS, LISTED.

NOT FOR CONSTRUCTION



LOCATION MAP
(NOT TO SCALE)



DEMOLITION NOTES:

- CONTRACTOR TO REMOVE ALL ITEMS WITHIN THE DEMOLITION AREAS.
- TREES TO BE CUT, REMOVED FROM SITE AND STUMPS REMOVED, NOT GROUND IN PLACE.
- BUILDINGS TO BE COMPLETELY REMOVED INCLUDING ALL FOUNDATIONS.
- CONTRACTOR TO COORDINATE WITH ALL UTILITY COMPANIES FOR PROPER SHUT OFF, DECOMMISSIONING, BULK-HEADING, ETC.OF ALL PUBLIC AND PRIVATE UTILITIES OR ARRANGE FOR REROUTING OF UTILITIES.
- CONTRACTOR TO WORK WITH CITY DPW FOR SHUTOFF, BULK-HEADING, AND ABANDONING OF ALL PUBLIC UTILITIES AND SERVICE LEADS.
- ALL REMOVED MATERIAL SHALL BE DISPOSED OF OFF SITE AND AT AN APPROVED FACILITY.
- MissDig Design Ticket Number B012982740-00B

DEMOLITION LEGEND:

- REMOVE EXISTING CURB
- DEMOLITION AREA
- REMOVE CONCRETE SURFACE

Executive:	AW
Manager:	JKA
Designer:	JKA
Quality Control:	AW
Section:	25
T-03-N R-10-E	

Professional Seal:



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Developed For:

WJ VENTURES, LLC

2060 COOLIDGE HWY
BERKLEY, MI 48072

248-246-7878

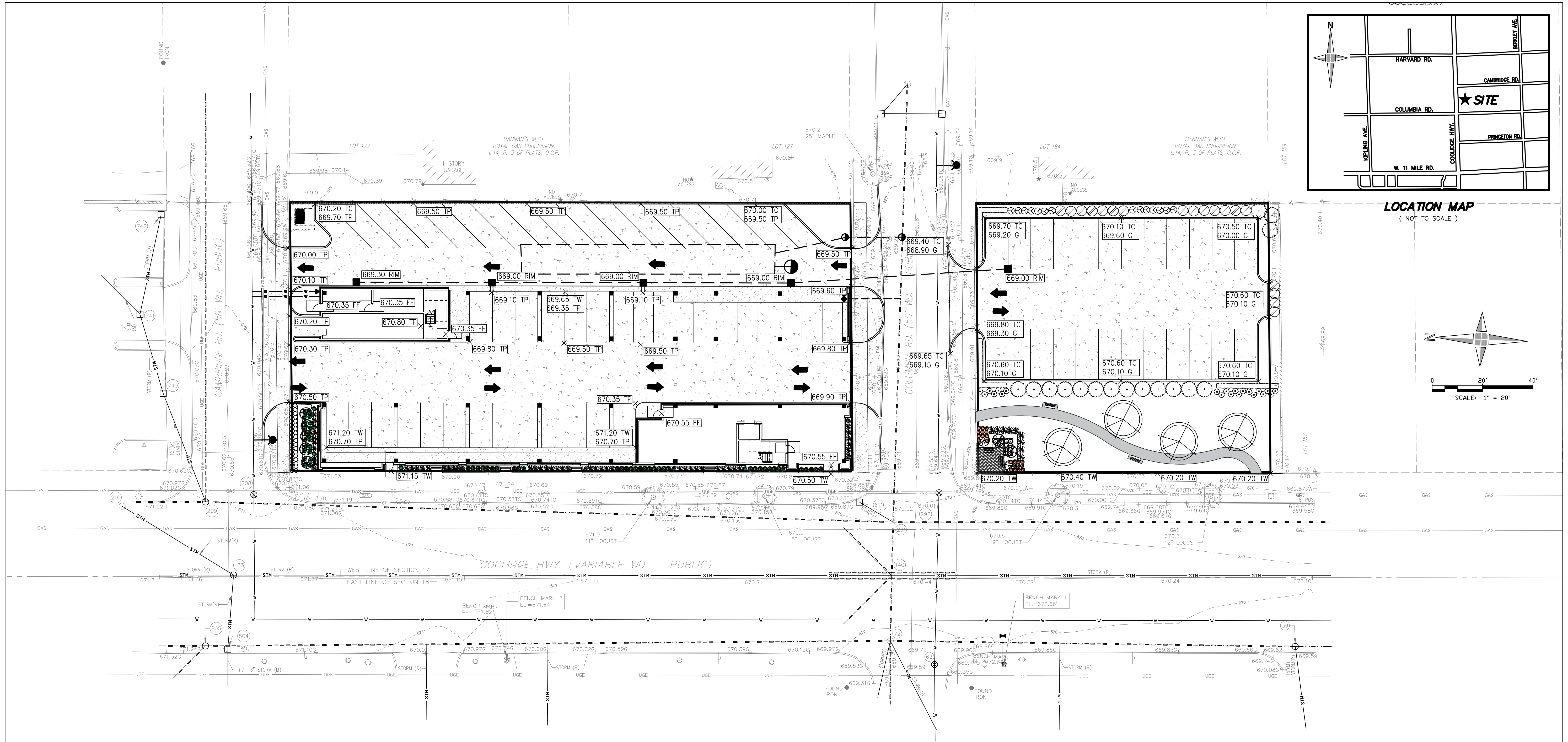
DEMOLITION PLAN

THE COLUMBIA

CITY OF BERKLEY
OAKLAND COUNTY
MICHIGAN

Date:	07.21.2023
Scale:	1" = 20'
Sheet:	C-300
Project:	20388.00

H:\2020\300\20388.00 Berkley_Lots\Design\CAD\2025 04 25 Final SP PUD submitt\600 GRADING Berkley Condo SP REV.dwg



LEGEND

670.00 TP	PR TOP OF PAVEMENT ELEV.
670.00 FG	PR FINISHED GRADE ELEV.
[Pattern]	PR 8" CONCRETE PAVEMENT
[Pattern]	PR 6" CONCRETE PAVEMENT
[Pattern]	PR 4" CONCRETE PAVEMENT

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2060 COOLIDGE HWY
BERKLEY, MI 48072

248-246-7878

GRADING and PAVING
PLAN

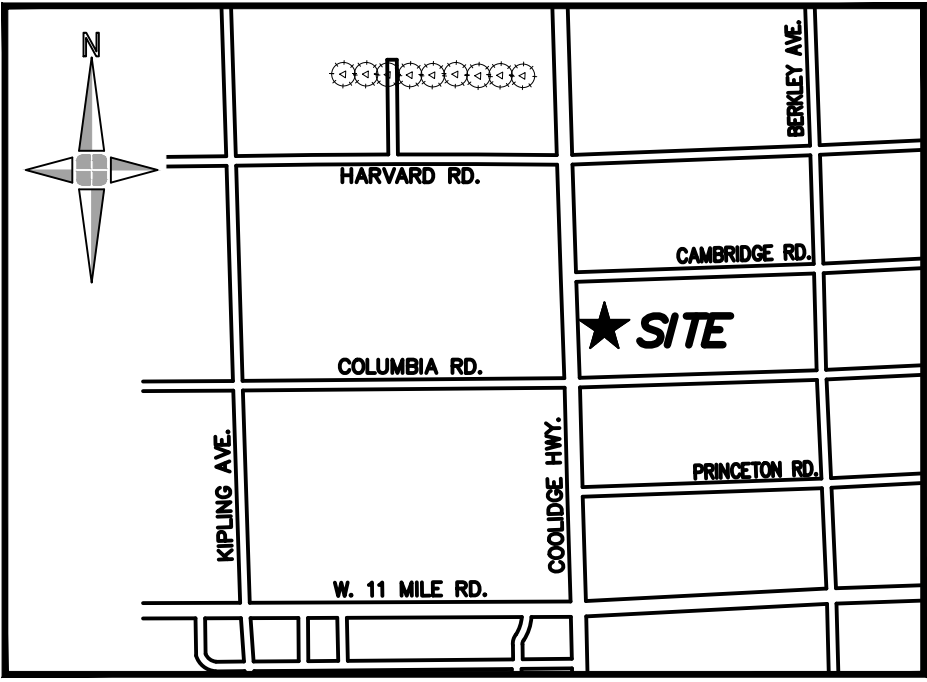
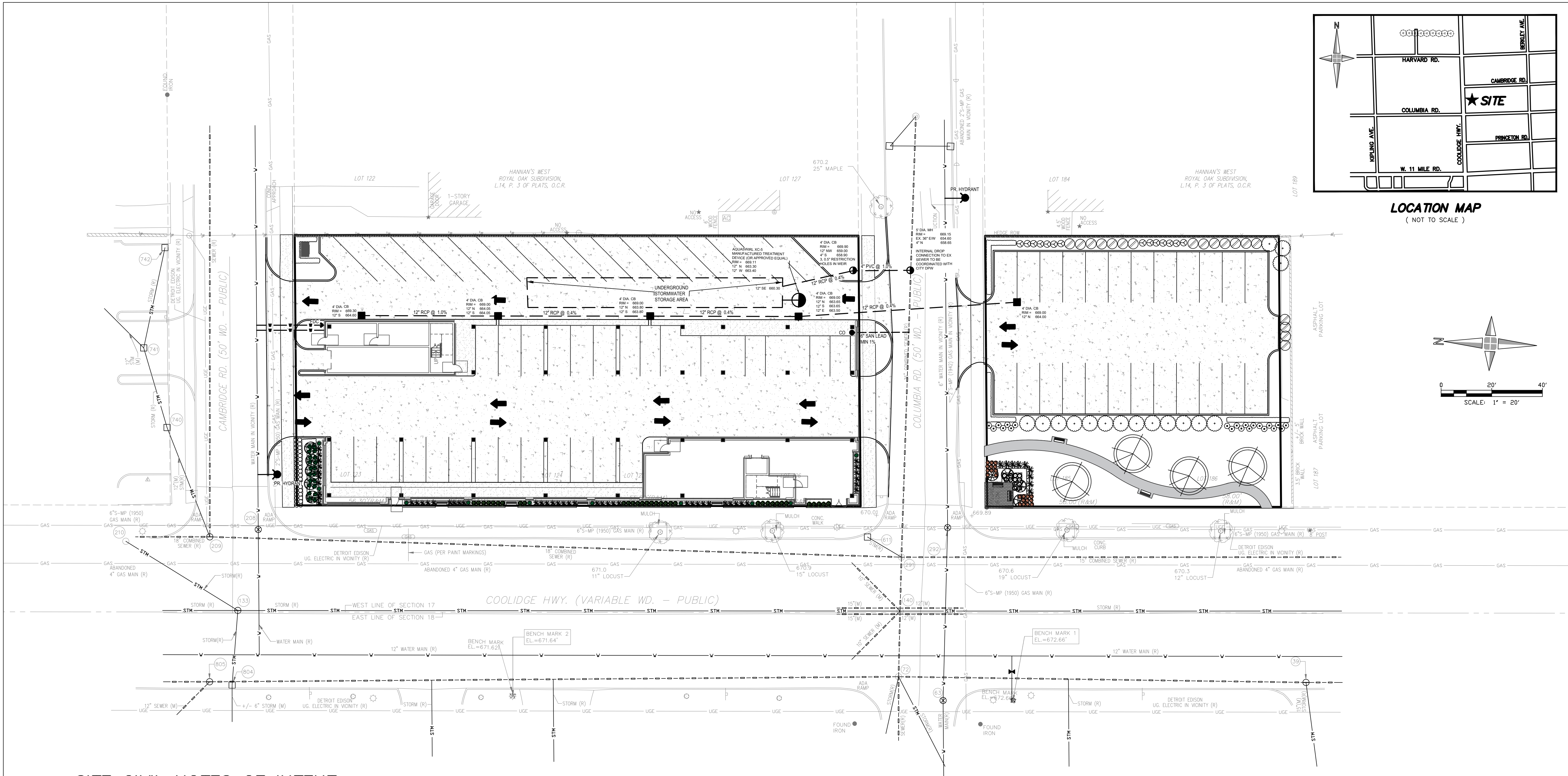
THE COLUMBIA

CITY OF BERKLEY
OAKLAND COUNTY
MICHIGAN

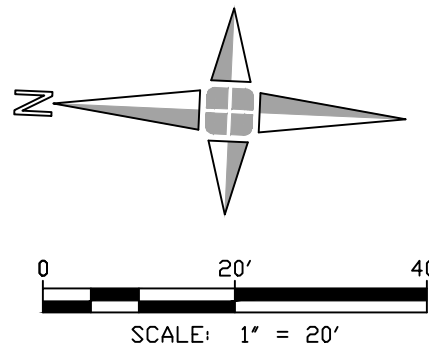
Date:	07.21.2023
Scale:	1" = 20'
Sheet:	C-600
Project:	20388.00

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H:\2020\3000\20388.00 Berkley Lots\Design\CAD\2025 04 25 Final SP PUD submit\C700 UTILITY Berkley Condo SP REV.dwg



LOCATION MAP
(NOT TO SCALE)



SITE CIVIL NOTES OF INTENT

- GRADING AND SESC: ALL GRADING AND SOIL EROSION & SEDIMENTATION CONTROL MEASURES WILL BE IMPLEMENTED AND STRICTLY ENFORCED THROUGHOUT CONSTRUCTION.
- SANITARY SERVICE: ALL EXISTING SANITARY SERVICES ARE PROPOSED TO BE ABANDONED VIA A SPOT LINER OR ANOTHER DPW APPROVED METHOD, WITHIN 2' OF THE MAIN. NEW SANITARY LEADS WILL BE INSTALLED AND CONNECTED WITH TWISTEE SEWER SADDLE PER DPW.
- WATER SERVICE: THE EXISTING WATER SERVICES WILL BE REMOVED TO THE CITY STOP BOXES OR PER DPW DIRECTION. A NEW DOMESTIC SERVICES WILL BE CONNECTED AND INSTALLED TO THE BUILDINGS. THE BUILDINGS WILL BE FIRE SUPPRESSED AND HAVE SEPARATE FIRE SERVICE LEADS INSTALLED.
- STREETS WILL BE RESTORED THAT ARE ASSOCIATED WITH UTILITY WORK.
- A TRAFFIC CONTROL PLAN WILL BE COORDINATED WITH CITY AND ADDED TO CONSTRUCTION PLANS FOR REVIEW AND APPROVAL.
- APPLICANT WILL COORDINATE / DISCUSS WITH CITY ON ANY ADDITIONAL UTILITY IMPROVEMENTS.
- ADDITIONAL FIRE HYDRANTS ARE PROPOSED TO PROVIDE APPROPRIATE FIRE COVERAGE.
- STORMWATER MANAGEMENT:
 - THE OAKLAND COUNTY WATER RESOURCES COMMISSIONER STORMWATER ENGINEERING DESIGN STANDARDS, UPDATED 11/22/2021, WILL BE FOLLOWED FOR PROPER DESIGN REQUIREMENTS. AND THE DESIGNED STORAGE WILL MEET OCWRC STANDARDS.
 - THIS SITE WILL ALSO UTILIZE A MECHANICAL TREATMENT DEVICE TO CAPTURE FLOATABLES AND FILTER OUT SEDIMENT. THIS UNIT WILL HAVE NDEP CERTIFICATION.
 - THE PROPOSED OUTLET CONTROL STRUCTURE WILL BE RESTRICTED AND DEWATER WITHIN ALLOWABLE TIMES.

STORMWATER SYSTEM NOTE:

STORMWATER MANAGEMENT SYSTEM WILL BE A PRIVATE SYSTEM UNDER THE MAINTENANCE OF THE PROPERTY OWNER. A MAINTENANCE AGREEMENT BETWEEN OWNER AND CITY OF BERKLEY WILL BE PROVIDED ONCE PLAN IS APPROVED. ALL NECESSARY EXHIBITS WILL BE INCLUDED WITH THE AGREEMENT.

LEGEND

	PR STORM SEWER
	PR STORM CATCH BASIN
	PR OUTLET CONTROL STRUCTURE
	PR MANUFACTURED TREATMENT DEVICE
	EX SANITARY LEAD
	EX WATER SERVICE
	FIRE HYDRANT

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Engineers
Surveyors
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Landscape Architects

1025 East Maple Road
Suite 100
Birmingham, MI 48009
p (248) 852-3100
f (313) 962-5068
www.giffelswebster.com

Executive: AW
Manager: JKA
Designer: JKA
Quality Control: AW
Section: 25
T-03-N R-10-E

Professional Seal:



Know what's below.
Call before you dig.

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08.29.2023	Submit #3 PUD to City
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04.26.2024	Revised PUD
06.04.2024	Rev. per PC Conditions for Approval
02.14.2025	Revised Plans for PUD Amendment
04.25.2025	City Council Submittal

Developed For:

WJ VENTURES, LLC

2060 COOLIDGE HWY
BERKLEY, MI 48072

248-246-7878

UTILITY PLAN

THE COLUMBIA

CITY OF BERKLEY
OAKLAND COUNTY
MICHIGAN

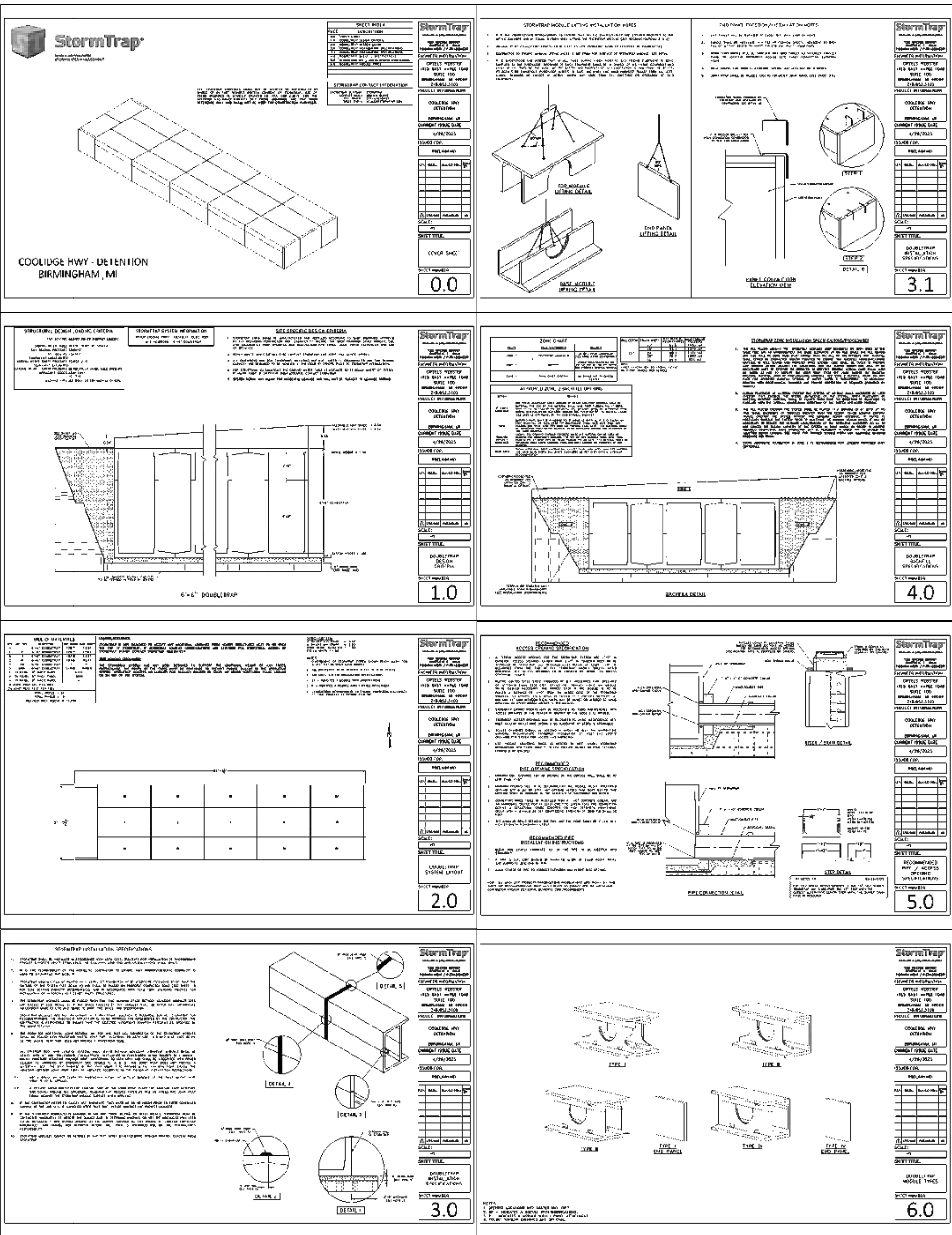
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Table 1. Aqua-Swift™ Microturbine Models and Associated MPEs

Model	Manhole Diameter (ft)	NJDEP 50% TSS Maximum Treatment Flow Rate, MGD (cfs)	50% Maximum Sediment Storage Area Volume (ft ³)
NC-2	2.5	0.07	2.46
NC-3	3.5	1.03	4.93
NC-4	4.5	1.86	7.92
NC-5	5.5	2.78	11.88
NC-6	6.5	3.88	16.99
NC-7	7.5	5.17	23.09
NC-8	8.5	6.64	29.38
NC-9	9.5	8.29	36.62
NC-10	10.5	10.12	45.30
NC-11	11.5	12.15	54.94
NC-12	12.5	14.45	66.36
NC-13	13	15.93	69.97



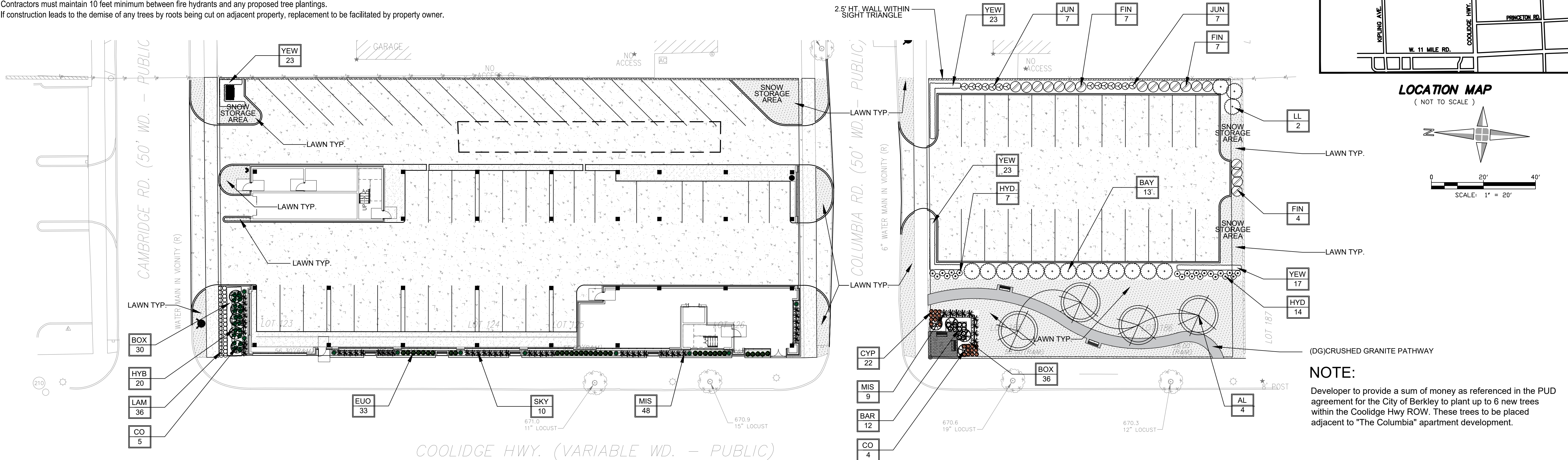
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GENERAL NOTES:

- All Construction shall conform to the current standards and specifications of local ordinances.
- All areas not built, landscaped, or paved upon shall be seeded or sodded. Terra seed system or approved equal is encouraged.
 - All lawn areas shall be irrigated with an underground automatic irrigation system. Irrigation heads shall be placed such that water will not hit any hard surfaces or structures.
 - Two (2) year guarantee period begins at Final Acceptance of landscape material by the City's representative, a landscape architect, and prior to the authorization of the occupancy permit. This is the first of two (1 of 2) required City inspections of the Landscape Plan installation.
 - A minimum of one cultivation in each month of June, July, and August shall be performed within the 2-year warranty period.
 - Building Department will not release construction bonds until City landscape architect has inspected the site and approved the installation of landscape plans. Final bond will not be released until the two (2) year guarantee expires and final inspection is made.
 - Owner is then responsible for care and maintenance of all plants upon return of bonds. All plant material is to be maintained for the natural life of the plant. No removal of any plants material is permitted without replacement.
 - Contractors must maintain 10 feet minimum between fire hydrants and any proposed tree plantings.
 - If construction leads to the demise of any trees by roots being cut on adjacent property, replacement to be facilitated by property owner.

LANDSCAPE CONTRACTOR'S "TWO-YEAR IRRIGATION REQUIREMENT PERIOD"

It shall be the responsibility of the landscape contractor to program and periodically adjust the irrigation system (provided by others) as required to ensure delivery of proper and adequate water supply to all plant material, and seeded lawn areas serviced by the same irrigation system, to ensure the establishment of healthy plant material and lawn for their first year of growth. Following the landscape contractor's two-year irrigation requirement period, the contractor shall provide the owner with suggested irrigation maintenance schedule for their use.



NOTE:

Developer to provide a sum of money as referenced in the PUD agreement for the City of Berkley to plant up to 6 new trees within the Coolidge Hwy ROW. These trees to be placed adjacent to "The Columbia" apartment development.

PLANT NOTES:

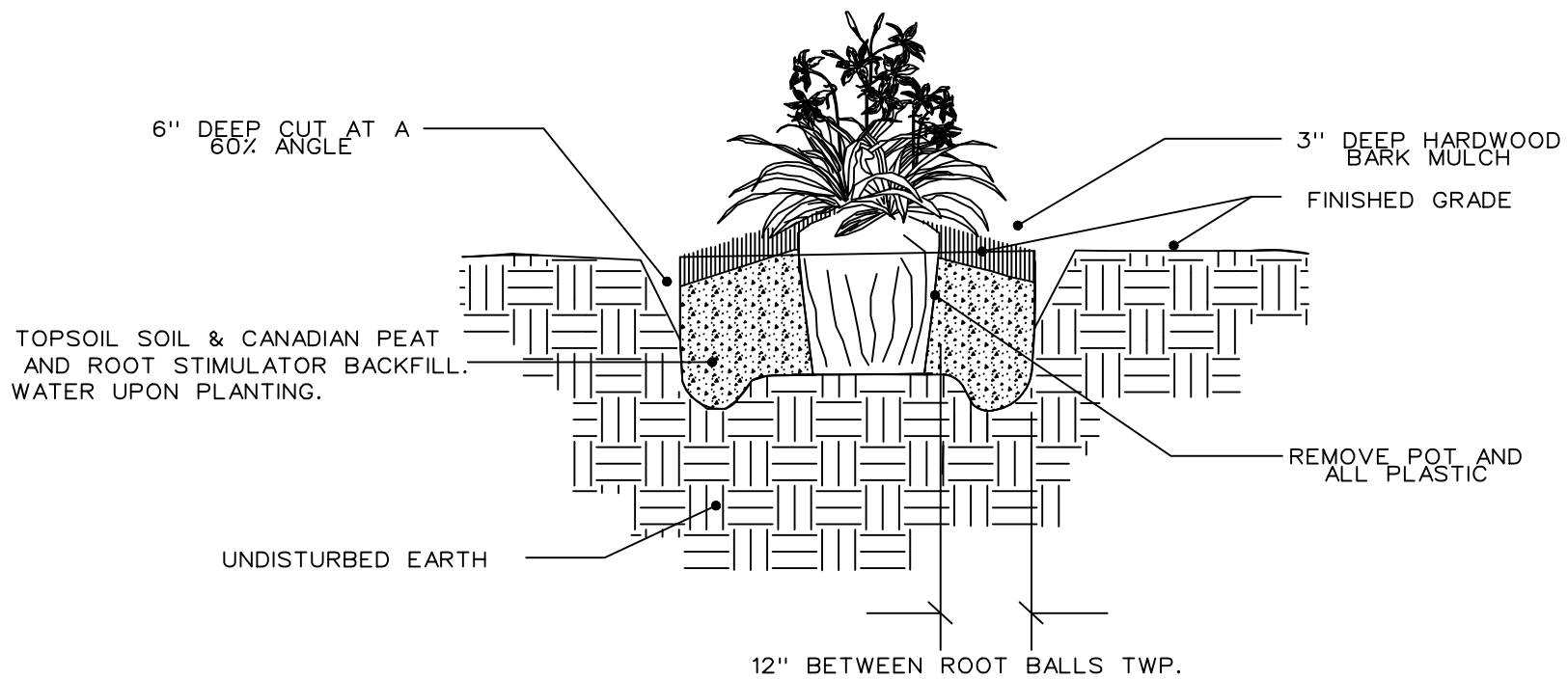
- Plant materials shall be sound, heathy vigorous, free from plant diseases and insects or their eggs, and shall have normal, healthy root systems. Caliper measurements shall be taken 6" above the ground level. All other measurements shall be in accordance with the latest edition of "American Standard for Nursery Stock" (ANSI Z-60.1 + A3002004).
- Planting bed soil for annuals shall be screened topsoil. To deter weed growth during the establishment of annual, & perennial bed, apply a pre-emergent ('Preen' or equal) after planting 2-3 times per growing season, at a rate per recommendation of product manufacturer.
- Planting pockets shall be no deeper than the height of the root ball, saucer shall be made on the edges of the plant pocket.
- All tree wrap shall be removed upon planting.
- Mulch shall be shredded hardwood bark natural in color, free from deleterious materials and suitable as a top dressing of planting beds and individual tree plantings.
- Trees shall be mulched with minimum of 4" deep hardwood bark mulch.
- Grass seed shall be certified turf grass seed complying with A.S.P.A. specifications, and free of weed seeds and undesirable native grasses. Seeded areas shall not be permitted to dry out. All lawn areas shall be fertilized and watered as required during the first growing season to maintain a dense and vigorous growing lawn.
- Plants shall be guaranteed for two complete growing seasons (24 months). Dead material shall be replaced as needed prior to the expiration of the guarantee period, in accordance with the local ordinance requirements. Failing plants should be replaced within 3 months of failing.
- Contractor shall provide in writing a list of recommended maintenance procedures for the first two (2) growing seasons.
- Remove top 1/3 of burlap on root ball or all if wrapped in plastic covering and/or nylon cord.
- Plant materials shall be used in compliance with the provisions of the local ordinance and shall be nursery grown, free of pests and diseases, hardy in this county, in conformance with the standards of the American Association of Nurserymen, and shall have passed inspections required under state regulations. In addition, plant materials shall be Upper Midwest/Great Lakes grown, No. 1 grade.
- Plant materials shall be planted within the annual planting window of March 15-Nov 15.
- Any plant substitutions must be approved in writing by the City prior to installation.

PLANT SCHEDULE

SYMBOL	ABBR	QUANTITY	BOTANICAL NAME	COMMON NAME	SIZE	NOTES
	CO	9	Quercus robur x crimsomidit	CRIMSON SPIRE OAK	3" cal.	B&B.
	AL	4	Tilia americana	AMERICAN LINDEN	3" cal.	B&B.
	BAY	13	Myrica pensylvanica	NORTHERN BAYBERRY	5 gal.	CONTAINER
	JUN	14	Juniperus chinensis 'blue point'	BLUE POINT JUNIPER	5 gal.	CONTAINER
	SKY	10	Juniperus scopulorum skyrocket	SKYROCKET JUNIPER	3 gal.	CONTAINER
	MIS	57	Miscanthus sinensis 'little kitten'	LITTLE KITTEN MAIDEN GRASS	3 gal.	CONTAINER
	BOX	66	Buxus 'Green Velvet'	GREEN VELVET BOXWOOD	3 gal.	CONTAINER
	FIN	18	Rhamnus frangula 'on williams'	FINELINE BARBERRY	3 gal.	CONTAINER
	BAR	12	Thuja 'Degroot's Spire'	GOLDEN RUBY BARBERRY	1 gal.	CONTAINER
	YEW	86	Buxus 'Green Velvet'	GREEN VELVET BOXWOOD	5 gal.	CONTAINER
	CYP	22	Chamaecyparis pisifera f. 'moj'	MOPS THREADLEAF CYPRESS	1 GAL	CONTAINER
	HYD	21	Hydrangea paniculata 'smnhph'	LITTLE LIME PUNCH HYDRANGEA	3 gal.	CONTAINER
	HYB	20	Hydrangea quercifolia 'Mundkin'	MUNCHKIN OAKLEAF HYDRANGEA	3 gal.	CONTAINER
	EUO	33	Euonymus fortunei emerald'n gold	EMERALD 'N GOLD EUONYMUS	1 gal.	CONTAINER
	LAM	36	Lamium maculatum 'orchid frost'	ORCHID FROST LAMIUM	1 gal.	CONTAINER

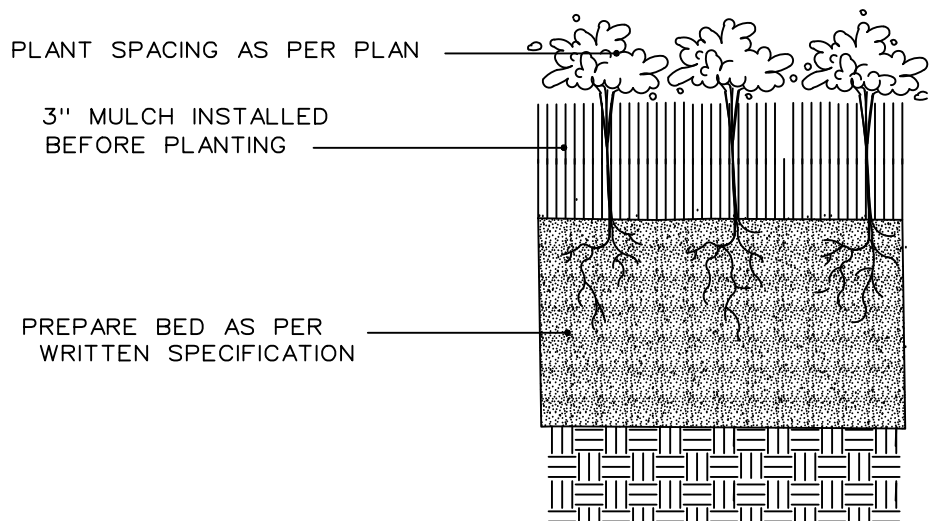
LAWN SEED MIX:

30% PERENNIAL RYGRASS
20% PARK KENTUCKY BLUEGRASS
45% CREEPING RED FESCUE
5% ANNUAL RYGRASS
4*/1000 S.F. SEEDING RATE



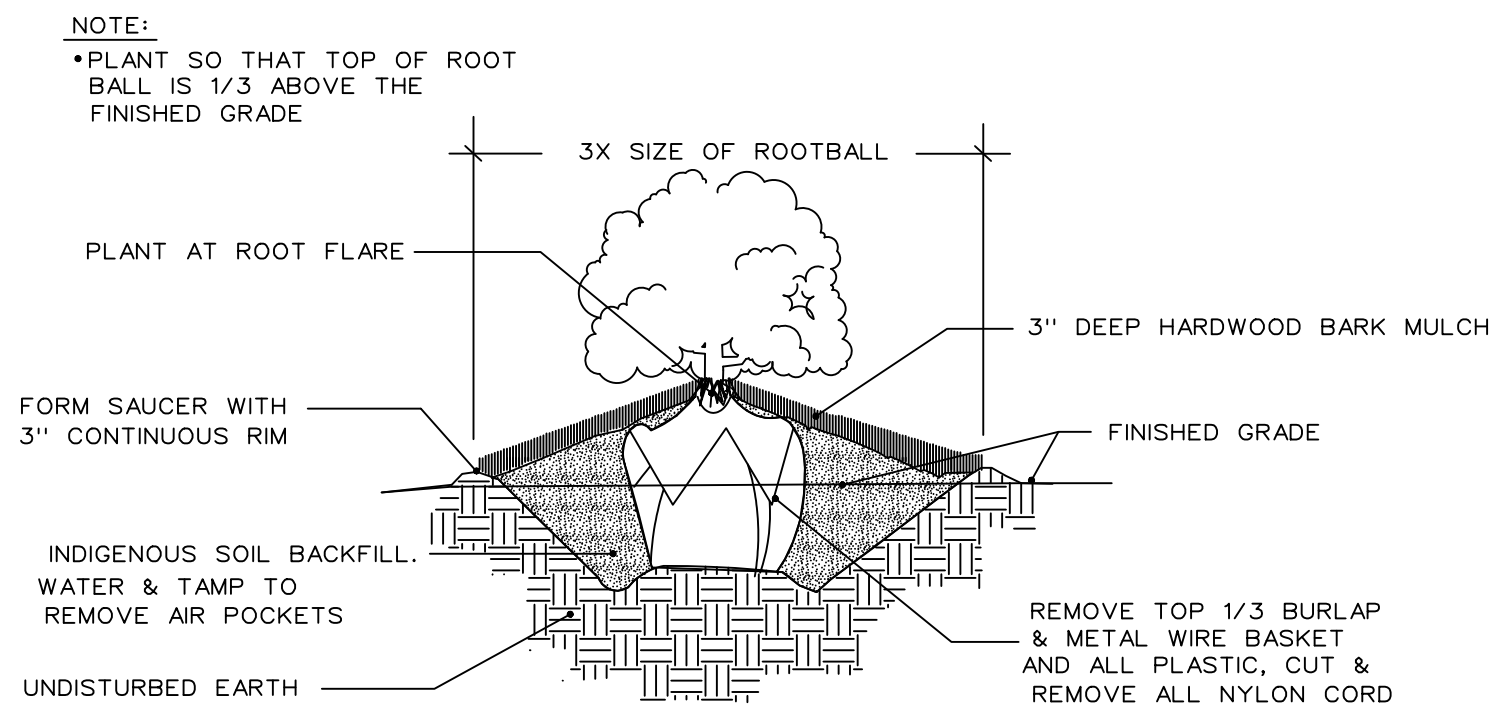
PERENNIAL PLANTING DETAIL

NO SCALE



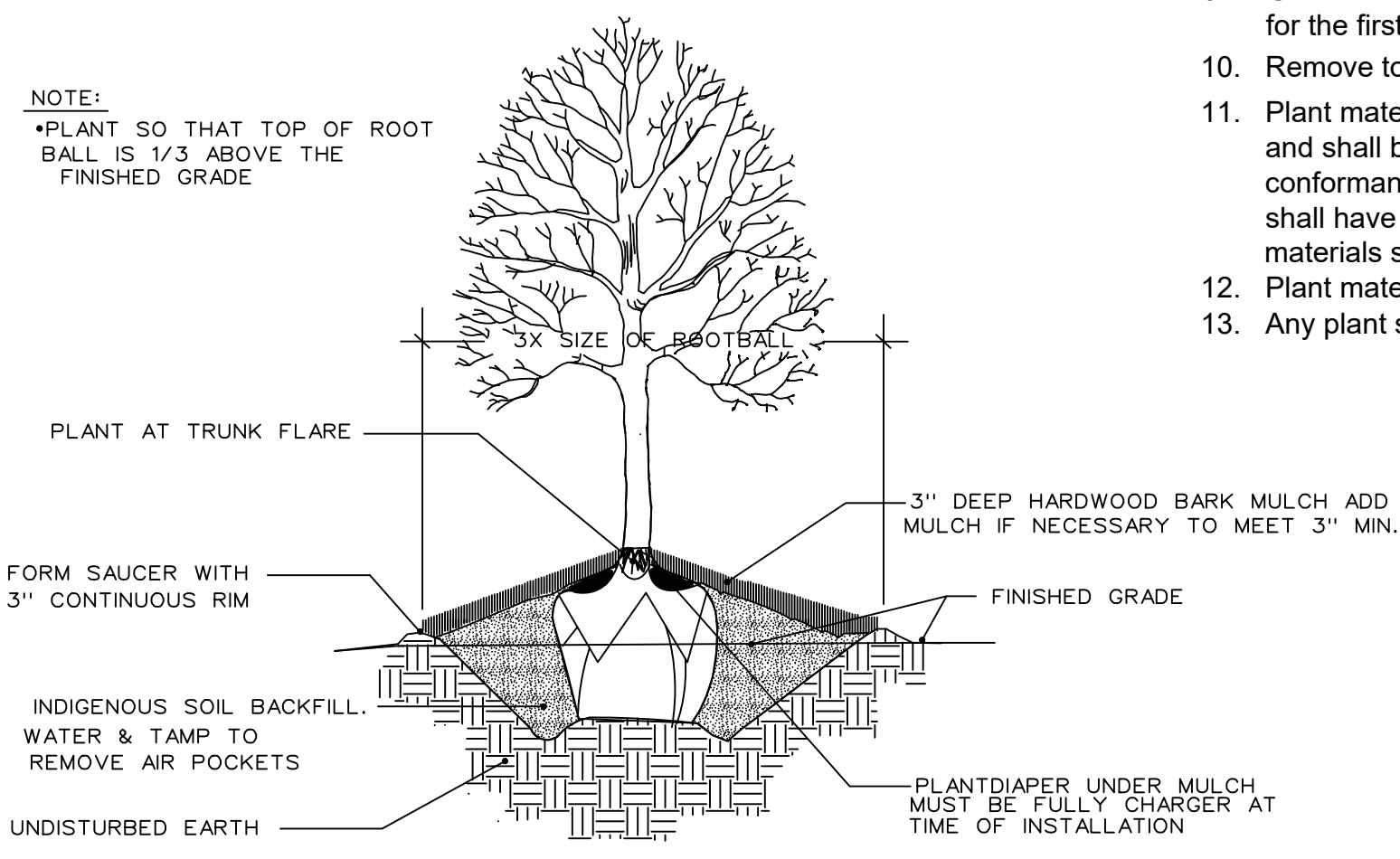
GROUNDCOVER PLANTING DETAIL

NO SCALE



SHRUB PLANTING DETAIL

NO SCALE



TREE PLANTING DETAIL

NO SCALE

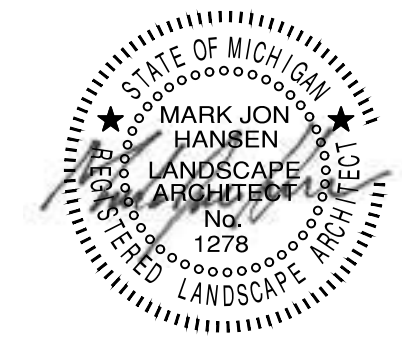
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Developed For:

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2060 COOLIDGE HWY
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248-246-7878

LANDSCAPE PLAN

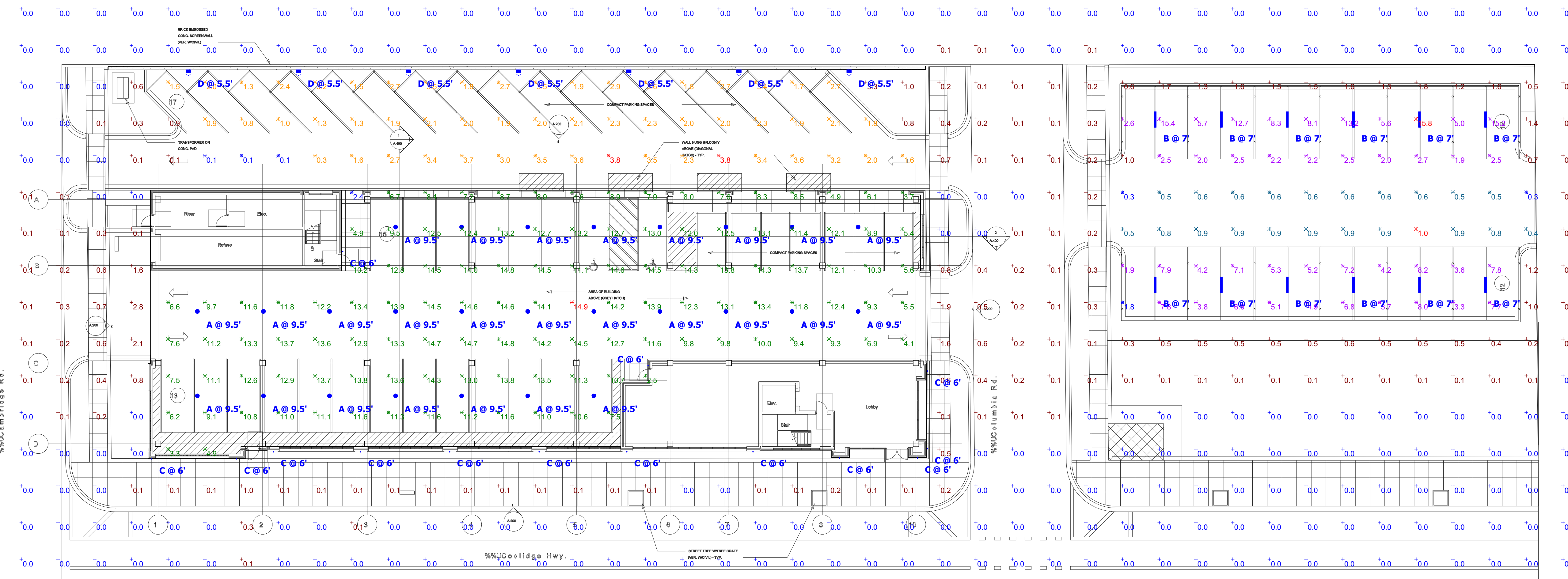
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Statistics						
Description	Symbol	Avg	Max	Min	Max/Min	Avg/Min
COVERED PARKING	✕	10.9 fc	14.9 fc	2.4 fc	6.2:1	4.5:1
OVERALL	+	2.8 fc	15.8 fc	0.0 fc	N/A	N/A
SECONDARY LOT	✕	0.7 fc	1.0 fc	0.3 fc	3.3:1	2.3:1
UNCOVERED PARKING	✕	2.2 fc	3.8 fc	0.1 fc	38.0:1	22.0:1
CARPORTS	✕	5.9 fc	15.8 fc	1.8 fc	8.8:1	3.3:1

Schedule						
Symbol	Label	QTY	Manufacturer	Description	Lamp	Mounting Height
✕	A	26	Lithonia Lighting	VCPG LED CANOPY LUMINAIRE 3000K	LED	9'-6"
+	B	12	Lithonia Lighting	FEM LED CANOPY LUMINAIRE 3000K	LED	7'-0"
○	C	14	HESS Licht+Form Copyright LUMCAL	Padua WALL SCONCE 3000K	LED	6'-0"
□	D	7	Lithonia Lighting	WDGE2 LED WALLPACK 3000K	LED	5'-0"

Energize with confidence!
Contact our EV Charging Team to source and specify industry leading hardware and software solutions.

Chris Aina
caina@gasserbush.com
734-460-4036
www.gasserbush.com

General Note
1. SEE SCHEDULE FOR LUMINAIRE MOUNTING HEIGHT.
2. CALCULATIONS ARE SHOWN IN FOOTCANDLES AT: 0' - 0"

THE ENGINEER AND/OR ARCHITECT MUST DETERMINE APPLICABILITY OF THE LAYOUT TO EXISTING / FUTURE FIELD CONDITIONS. THIS LIGHTING LAYOUT REPRESENTS ILLUMINATION LEVELS CALCULATED FROM LABORATORY DATA TAKEN UNDER CONTROLLED CONDITIONS IN ACCORDANCE WITH ILLUMINATING ENGINEERING SOCIETY APPROVED METHODS. ACTUAL PERFORMANCE OF ANY MANUFACTURER'S LUMINAIRE MAY VARY DUE TO VARIATION IN ELECTRICAL VOLTAGE, TOLERANCE IN LAMPS, AND OTHER VARIABLE FIELD CONDITIONS. MOUNTING HEIGHTS INDICATED ARE FROM GRADE AND/OR FLOOR UP.

THESE LIGHTING CALCULATIONS ARE NOT A SUBSTITUTE FOR INDEPENDENT ENGINEERING ANALYSIS OF LIGHTING SYSTEM SUITABILITY AND SAFETY. THE ENGINEER AND/OR ARCHITECT IS RESPONSIBLE TO REVIEW FOR MICHIGAN ENERGY CODE AND LIGHTING QUALITY COMPLIANCE.

UNLESS EXEMPT, PROJECT MUST COMPLY WITH LIGHTING CONTROLS REQUIREMENTS DEFINED IN ASHRAE 90.1 2013. FOR SPECIFIC INFORMATION CONTACT GBA CONTROLS GROUP AT ASG@GASSERBUSH.COM OR 734-266-6705.

Plan View
Scale - 1" = 16ft



Mounting Height Note
MOUNTING HEIGHT IS MEASURED FROM GRADE TO SKY SIDE OF FIXTURE. POLE HEIGHT SHOULD BE CALCULATED AS THE MOUNTING HEIGHT LESS BASE HEIGHT.

Ordering Note
FOR INQUIRIES CONTACT GASSER BUSH AT
QUOTES@GASSERBUSH.COM OR 734-266-6705.

Drawing Note
THIS DRAWING WAS GENERATED FROM AN ELECTRONIC IMAGE AND FOR ESTIMATION PURPOSE ONLY. LAYOUT TO BE VERIFIED IN FIELD BY OTHERS.

Alternates Note
ALTERNATE LIGHTING FIXTURES WILL NOT MEET CITY ORDINANCE COMPLIANCE DUE TO THE PRECISE OPTICAL AND OUTPUT PERFORMANCE REQUIRED FOR THESE FIXTURES. ALTERNATE LIGHTING PROPOSALS MUST BE RECALCULATED AND RESUBMITTED TO THE CITY FOR APPROVAL. CONTACT LAYOUTS@GASSERBUSH.COM FOR ASSISTANCE WITH ALTERNATE OPTIONS IF NEEDED.

VC PG LED
Parking Garage

Specifications
Diameter: 19"
Height: 3.75"
Weight: 18 lbs
Finish: with no options

Introduction
The all new VCPG LED (Visually Comfortable Parking Garage) luminaire is designed to bring glare control, optical performance and energy savings into one package. The recessed lens design of VCPG LED minimizes high angle glare, while its precision molded acrylic lens eliminates LED pixelation and delivers the required minimums, verticals and uniformity. The dedicated up-light module option reduces the contrast between the luminaire and the ceiling creating a more visually comfortable environment. The VCPG LED delivers up to 87% in energy savings when replacing 175W metal halide luminaires. With over 100,000 hour life expectancy (12+ years of 24/7 continuous operation), the VCPG LED luminaire provides significant maintenance savings over traditional luminaires.

Ordering Information
EXAMPLE: VCPG LED V4 P4 40K 70CRI TSM MVOLT SRM DNAXD

Series	LED Light Engines	Package	Color Temperature	Color Rendering Index	Distribution	Voltage	Mounting
VCPG LED	V4	P1	30K	90CRI	TSM	MVOLT	
	V4	P2	35K	90CRI	TSM	120	
	V4	P3	40K	90CRI	TSM	200	
	V4	P4	50K	90CRI	TSM	240	
	V4	P5	50K	90CRI	TSM	277	
	V4	P6	50K	90CRI	TSM	347	
	V4	P7	50K	90CRI	TSM	480	

Shipped included
PM Instant mount standard (24 inch length supply leads)
SRM Surface mount (24 inch length supply leads)
ARM Arm mount (see DIMENSIONS section for arm to wall)

Shipped separately
TK "Microtension" mount

Options
A+ Capable options indicated by shaded background

Shipped installed
UPL1 Up-light 500 lumens
UPL2 Up-light 700 lumens
EMRC Emergency battery backup, Certified in CA Title 20 MAZD05 (90K, 20% max)
E200W Emergency battery backup, Certified in CA Title 20 MAZD05 (100K, 9% max)
HA High ambient (50°C, only P1-P4)
SF Single face (100K, 277K, 347K)
DF Double face (200K, 480K, 600K)
SPD100V 100V lamp length
LSD36 36in (36) lead length
LSD72 72in (72) lead length
LSD144 144in (144) lead length
DMG External 0-10V dimmable (no control)
SP Lamp power screws

Shipped separately
HS Wire Guard
BS Bird Shield
HS House Side Shield
BS Bird Spikes

Standard Sensors/Controls
PIR Motion/ambient sensor for 8-15' mounting heights
PIR Motion/ambient sensor for 15-30' mounting heights
PIRSCV Motion/ambient sensor for 15-30' mounting heights, pre-programmed to 30 and 35% light output
PIRHSVCV Motion/ambient sensor for 15-30' mounting heights, pre-programmed to 30 and 35% light output

Networked Sensors/Controls
NLSA01 40W Wireless enabled motion/ambient sensor for 8-15' mounting heights
NLSA02 40W Wireless enabled motion/ambient sensor for 15-30' mounting heights
NLSA03 40W Wireless enabled, UL204 Listed motion/ambient sensor for 8-15' mounting heights
NLSA04 40W Wireless enabled, UL204 Listed motion/ambient sensor for 15-30' mounting heights

Finish
DNAXD White
DNAXD Natural aluminum
DNAXD Dark bronze
DNAXD Black

Note: Product will be shipped in job packs of 100 per bin. The remaining parts will be shipped in unit packs.

Items marked by a shaded background qualify for the Design Select program and ship in 15 days or less. To learn more about Design Select, visit www.acuitybrands.com/designselect.
*See ordering tree for details

FEATURES & SPECIFICATIONS
INTENDED USE — A general purpose and energy-efficient surface-mounted or suspended LED fixture, suitable for wet, damp and/or cold locations. For vapor tight demanding environments where moisture or dust is a concern and where relatively low fixture mounting heights and wide fixture spacing are common. Not for use or installation in direct outdoor sunlight. Must be installed under canopy or covered ceiling. For direct sunlight installations, please refer to the 115 product family. Typical applications include industrial facilities, parking garages, retail malls, multi-purpose venues, garden centers, and food processing. **Certain airborne contaminants can diminish the integrity of acrylic and/or polycarbonate.** Click here for [Acrylic Polycarbonate Compatibility table for suitability](#).
Certain airborne contaminants may adversely affect the functioning of LEDs and other electronic components, depending on various factors such as concentrations of the contaminants, ventilation, and temperature at the end-user location. Click here for [a list of substances that may not be suitable for interaction with LEDs and other electronic components](#).
CONSTRUCTION — One-piece 50A fiberglass housing with integral perimeter channel utilizing continuous poured-in-place NEMA 4X gasket. Approved for thorough washing. Capable polymer latches are standard. Stainless steel latches (416) available as an option for food processing or more demanding applications. Power connection is easily accomplished through pre-drilled holes.
OPTICS — Injection molded, acrylic lens (0.80" thick) provides high impact-resistance comparable to 300% DR. A UV stabilized polycarbonate diffuser is available (0.80" thick) in clear or frosted for additional impact strength where vandalism protection is desired. Expected service life of 40,000 hours at 80% lumen maintenance (L80); predicted life of more than 100,000 hours.
ELECTRICAL — Utilizes high-efficiency LEDs mounted to cure-on boards. High-efficiency drivers operate 120-277 MVOLT and 347-480 MVOLT offered with 50-watt dimming, dim-to 10%, Standard Luminaire Surge Protection Level (ALV) Surge Rated per ANSI C82.7-75-2015.
INSTALLATION — A pair of stainless steel surface mount brackets (SMB) are included unless another mounting option is chosen, allowing for surface ceiling or suspension mount applications using included ball with aircraft cable or chain. Optional pair of dual pendant mount brackets (DPMB) are available for surface ceiling or suspension mount applications using either 3/8" threaded rod or included ball with aircraft cable or chain. Optional pair of angle-mounting brackets (AMGB) for horizontal wall mount applications.
LISTINGS — CSA Certified to UL and UL Standards. Suitable for wet location. IP65, IP66 and IP69K (optional) rated. NSF Splash Zone 2 and Non-Food Zone rated. NEMA 4X rated. Sensors maintain IP65 and IP66 only. See chart on page 5 for Ambient Temperature.
DesignLights Consortium (DLC) Premium qualified product and DLC qualified product. Not all versions of this product may be DLC Premium qualified or DLC qualified. Please check the DLC Qualified Product List at www.designlights.org/DLP to confirm which versions are qualified.
GOVERNMENT PROCUREMENT — BAA — Product with the BAA option qualifies as a domestic and product under the Buy American Act as implemented in the FAR and DFARS. Product with the BAA option also qualifies as manufactured in the United States under DOT Buy America regulations.
BABA — Build America Buy America Product with the BABA option also qualifies as produced in the United States under the definitions of the Build America, Buy America Act.
Please refer to www.acuitybrands.com/buy-america for additional information.
WARRANTY — 5-year limited warranty. This is the only warranty provided and no other statements in this specification sheet create any warranty of any kind. All other express and implied warranties are disclaimed. Complete warranty terms located at: www.acuitybrands.com/support/warranty-terms-and-conditions.
NOTE: Actual performance may differ as a result of end-user environment and application. All values are design or typical values, measured under laboratory conditions at 25 °C. Specifications subject to change without notice.

Stock configurations are offered for shorter lead times:

Standard Part Number	Stock Part Number
FEM 140X100 (LUM) MD MVOLT 4210 40K DCB	FEM 140 4L MVOLT
FEM 140X100 (LUM) MD MVOLT 4210 50K DCB	FEM 140 4L MVOLT 5K

Items marked by a shaded background qualify for the Design Select program and ship in 15 days or less. To learn more about Design Select, visit www.acuitybrands.com/designselect.
*See ordering tree for details

PADUA 100
Wall Mounted Luminaire

TYPE C

DESCRIPTION
PADUA combines simple geometric shapes to produce an understated form that enhances contemporary architecture. The cylindrical housing is extruded aluminum, cradled by the concave detailing of the fabricated aluminum wall mounting bracket and ballast shaft. The fully shielded luminaire emits zero uplight. It is suitable for LED lighting zones 1 through 4 and is Dark Sky compliant. Integral universal LED driver. All hardware is stainless steel.

ORDERING INFORMATION

LUMINAIRE	MODEL	CCT	VOLTAGE	FINISH (Primar®)	OPTION
PADUA 100	30K	3000K	UNV	Dark Grey	AP Adapter Plate For Mounting Over 3.5' to 4' Groggless Bar
	40K	4000K		GG Graphite Grey	
				SG Silver Grey	
				BL Black	
				DB Dark Bronze	
				CC CustomRAL Color	

EXPERIENCE BRANDS

www.hessAmerica.com
For other Experience Brands companies, please visit www.experiencebrands.com.
We continue to offer to offer the best product possible, we reserve the right to change, without notice, specifications or materials.
Technical specification sheets that appear on www.hessAmerica.com are the most recent ones possible.

Rev 20220808

WDGE2 LED
Architectural Wall Sconce
Precision Refractive Optic

TYPE D

Specifications
Depth (D1): 7"
Depth (D2): 1.5"
Height: 9"
Width: 11.5"
Weight: 13.5 lbs
(without options)

Introduction
The WDGE2 family is designed to meet specifier's every wall-mounted lighting need in a widely accepted shape that blends with any architecture. The clean rectangular design comes in four sizes with lumen packages ranging from 1,200 to 25,000 lumens, providing a true site-wide solution. Embedded with nLight® AIR wireless controls, the WDGE2 family provides additional energy savings and code compliance. WDGE2 with industry leading precision refractive optics provides great uniform distribution and optical control. When combined with multiple integrated emergency battery backup options, including an 18W cold temperature option, the WDGE2 becomes the ideal wall-mounted lighting solution for pedestrian scale applications in any environment.

Ordering Information
EXAMPLE: WDGE2 LED P3 40K 80CRI T3M MVOLT SRM DBXBD

Series	Package	Color Temperature	CR	Distribution	Voltage	Mounting	Shipped included	Shipped separately
WDGE2 LED	P0	27K	2700K	70CRI	T15	Type I Short	MVOLT	AW5 3300h Architectural wall space
	P1	30K	3000K	80CRI	T24	Type I Medium	347V	AW5 3300h Architectural wall space
	P2	40K	4000K	90CRI	LM Limited	Type I Medium	480V	AW5 3300h Architectural wall space
	P3	50K	5000K	90CRI	T14	Type I Medium		AW5 3300h Architectural wall space
	P4	AMP	Amber	TTM	Forward Throw Medium			AW5 3300h Architectural wall space

Options
E100W Emergency battery backup, Certified in CA Title 20 MAZD05 (100K, 9% max)
E200W Emergency battery backup, Certified in CA Title 20 MAZD05 (100K, 9% max)
PE Photocell/Battery Type
DMC 0-10V dimming with push button outside fixture (for use with an external control, ordered separately)
PIRHSVCV Motion/ambient sensor for 15-30' mounting heights with photocell pre-programmed for dusk to dawn operation
KCE Battery control only for back box (PBB), total 4 energy points
CEE Coastal Construction

Standard Sensors/Controls
PIR Bi-level (100/10%) motion sensor for 8-15' mounting heights. Intended for use on switched circuits with external dusk to dawn switching.
PIRHSVCV Bi-level (100/10%) motion sensor for 15-30' mounting heights. Intended for use on switched circuits with external dusk to dawn switching.
PIRHSVCV Bi-level (100/10%) motion sensor for 15-30' mounting heights with photocell pre-programmed for dusk to dawn operation.
PIRHSVCV Bi-level (100/10%) motion sensor for 15-30' mounting heights with photocell pre-programmed for dusk to dawn operation.


Networked Sensors/Controls
NLSA01 40W Wireless enabled motion/ambient sensor for 8-15' mounting heights.
NLSA02 40W Wireless enabled motion/ambient sensor for 15-30' mounting heights.
NLSA03 40W Wireless enabled, UL204 Listed motion/ambient sensor for 8-15' mounting heights.
NLSA04 40W Wireless enabled, UL204 Listed motion/ambient sensor for 15-30' mounting heights.
NLSA05 40W Wireless enabled, UL204 Listed motion/ambient sensor for 15-30' mounting heights.
NLSA06 40W Wireless enabled, UL204 Listed motion/ambient sensor for 15-30' mounting heights.
NLSA07 40W Wireless enabled, UL204 Listed motion/ambient sensor for 15-30' mounting heights.
NLSA08 40W Wireless enabled, UL204 Listed motion/ambient sensor for 15-30' mounting heights.
NLSA09 40W Wireless enabled, UL204 Listed motion/ambient sensor for 15-30' mounting heights.
NLSA10 40W Wireless enabled, UL204 Listed motion/ambient sensor for 15-30' mounting heights.
NLSA11 40W Wireless enabled, UL204 Listed motion/ambient sensor for 15-30' mounting heights.
NLSA12 40W Wireless enabled, UL204 Listed motion/ambient sensor for 15-30' mounting heights.
NLSA13 40W Wireless enabled, UL204 Listed motion/ambient sensor for 15-30' mounting heights.
NLSA14 40W Wireless enabled, UL204 Listed motion/ambient sensor for 15-30' mounting heights.
NLSA15 40W Wireless enabled, UL204 Listed motion/ambient sensor for 15-30' mounting heights.
NLSA16 40W Wireless enabled, UL204 Listed motion/ambient sensor for 15-30' mounting heights.
NLSA17 40W Wireless enabled, UL204 Listed motion/ambient sensor for 15-30' mounting heights.
NLSA18 40W Wireless enabled, UL204 Listed motion/ambient sensor for 15-30' mounting heights.
NLSA19 40W Wireless enabled, UL204 Listed motion/ambient sensor for 15-30' mounting heights.
NLSA20 40W Wireless enabled, UL204 Listed motion/ambient sensor for 15-30' mounting heights.

Finish
DBXBD Dark bronze
DBXBD Natural aluminum
DBXBD White
DBXBD Dark bronze
DBXBD Forward dark bronze
DBXBD Forward black
DBXBD Forward natural aluminum
DBXBD Forward white
DBXBD Forward sandstone

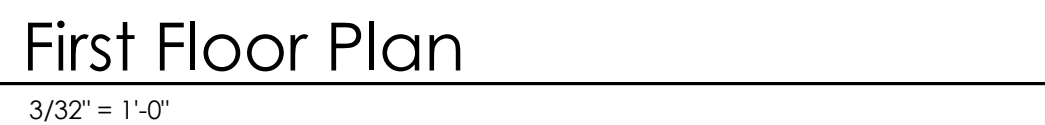
Items marked by a shaded background qualify for the Design Select program and ship in 15 days or less. To learn more about Design Select, visit www.acuitybrands.com/designselect.
*See ordering tree for details

A circular professional seal for the State of Michigan. The outer ring contains the text "STATE OF MICHIGAN" at the top and "LICENSED ARCHITECT" at the bottom, separated by stars. The center of the seal contains the name "JASON P. KRIEGER", the title "ARCHITECT", and the license number "No. 53578". A large, stylized signature is written across the seal, overlapping the text.

North

A circular compass rose with a horizontal line and a vertical line intersecting at the center. The top of the vertical line is labeled 'North', the bottom is labeled 'South', the right end of the horizontal line is labeled 'East', and the left end is labeled 'West'. The lines are thick and black.

A.100



Parking Schedule	
Count	
	69
	69

RATIO: 1.35 SPACES / UNIT

WJ Ventures, LLC
2060 Coolidge Hwy. Berkley, MI
48072

The Columbia
2476 Columbia Rd.
Berkley, MI 48072

[illegible]

Do not scale drawings. Use
calculated dimensions only.
Verify existing conditions in
field.

Typical Floor Plan
(Second - Third)

Sheet Number: _____

Unit Schedule	
Name	Count
02 - Second Floor	
A1 - Studio	4
A2 - Studio	1
B1 - 1 Bedroom	8
B2 - 1 Bedroom	1
B2.1 - 1 Bedroom	1
B3 - 1 Bedroom	1
B3.1 - 1 Bedroom	1

03 - Third Floor	
A1 - Studio	4
A2 - Studio	1
B1 - 1 Bedroom	8
B2 - 1 Bedroom	1
B2.1 - 1 Bedroom	1
B3 - 1 Bedroom	1
B3.1 - 1 Bedroom	1

04 - Fourth Floor	
A1 - Studio	4
A2 - Studio	1
B1 - 1 Bedroom	8
B2 - 1 Bedroom	1
B2.1 - 1 Bedroom	1
B3 - 1 Bedroom	1
B3.1 - 1 Bedroom	1

Total Units	5
-------------	---

Unit Percentage		
Occupancy	Count	Unit %
1 Bedroom	36	70.6%
Studio	15	29.4%
Total Units	51	

Unit SF	
Name	Area
Unit A1	623 SF
Unit A2	575 SF
Unit B1	739 SF
Unit B2	683 SF
Unit B2.1	683 SF
Unit B3	821 SF
Unit B3.1	701 SF

Gross Area	
Area	
01 - First Floor	14705 SF
02 - Second Floor	14203 SF
03 - Third Floor	14203 SF
04 - Fourth Floor	14203 SF
	57313 SF

Resi. Net Area	
Area	
02 - Second Floor	11785 SF
03 - Third Floor	11685 SF
04 - Fourth Floor	11685 SF
	35154 SF

Parking Schedule
Count
69

RATIO: 1.35 SPACES / UNIT
$$3/32'' = 1'-0''$$

WJ Ventures, LLC
2060 Coolidge Hwy. Berkley, MI
48072

The Columbia
2476 Columbia Rd.
Berkley, MI 48072

[illegible]

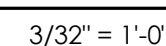
A circular professional seal for the State of Michigan. The outer ring contains the text "STATE OF MICHIGAN" at the top and "LICENSED ARCHITECT" at the bottom, separated by stars. The inner circle contains the name "JASON P. KRIEGER" and the license number "No. 53578". A stylized signature is written across the seal.

Do not scale drawings. Use
calculated dimensions only.
Verify existing conditions in
field.

Fourth Floor Plan

Sheet Number:

A.102



Unit Schedule	
Name	Count
02 - Second Floor	
A1 - Studio	4
A2 - Studio	1
B1 - 1 Bedroom	8
B2 - 1 Bedroom	1
B2.1 - 1 Bedroom	1
B3 - 1 Bedroom	1
B3.1 - 1 Bedroom	1

03 - Third Floor	
A1 - Studio	4
A2 - Studio	1
B1 - 1 Bedroom	8
B2 - 1 Bedroom	1
B2.1 - 1 Bedroom	1
B3 - 1 Bedroom	1
B3.1 - 1 Bedroom	1

04 - Fourth Floor	
A1 - Studio	4
A2 - Studio	1
B1 - 1 Bedroom	8
B2 - 1 Bedroom	1
B2.1 - 1 Bedroom	1
B3 - 1 Bedroom	1
B3.1 - 1 Bedroom	1

Total Units	51
-------------	----

Unit Percentage		
Occupancy	Count	Unit %
1 Bedroom	36	70.6%
Studio	15	29.4%
Total Units	51	

Unit SF	
Name	Area
Unit A1	623 SF
Unit A2	575 SF
Unit B1	739 SF
Unit B2	683 SF
Unit B2.1	683 SF
Unit B3	821 SF
Unit B3.1	701 SF

Gross Area	
Area	
01 - First Floor	14705 SF
02 - Second Floor	14203 SF
03 - Third Floor	14203 SF
04 - Fourth Floor	14203 SF
	57313 SF

Resi. Net Area	
Area	
02 - Second Floor	11785 SF
03 - Third Floor	11685 SF
04 - Fourth Floor	11685 SF
	35154 SF

Parking Schedule
Count
69

RATIO: 1.35 SPACES / UNIT

Roof Plan

WJ Ventures, LLC

The Columbia
2476 Columbia Rd.
Berkley, MI 48072

[illegible]

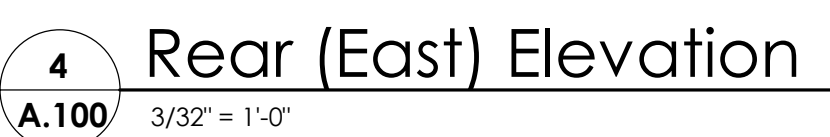
Do not scale drawings. Use
calculated dimensions only.
Verify existing conditions in
field.

Sheet Title:
Elevations

Project Number

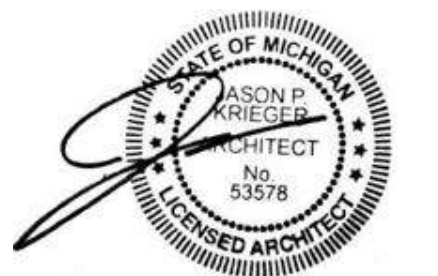
$$3/32'' = 1'-0''$$

A.200



[illegible]

Seal:



Note:

Do not scale drawings. Use calculated dimensions only. Verify existing conditions in field.

North Arrow:

Sheet Title:

Rendered
Elevations

Project Number: _____
Project Number

Scale:

Sheet Number: _____

A.201



Front (West) Elevation



Left (North) Elevation

Right (South) Elevation



Rear (East) Elevation

PRELIMINARY NOT FOR CONSTRUCTION

S.S. PINS @ 24" O.C.

CONC. CAP W/ WASH

THRU WALL MASONRY FLASHING

BRICK EMBOSSED CONCRETE

6'-0"

2" 8" 2"

3'-0"

1'-0"

TRENCH FDN W/ REBAR T&B

1" = 1'-0"







February 14, 2025

Kristen Kapelanski
City of Berkley Community Development Director
3338 Coolidge Hwy.
Berkley, MI 48072

Re: The Columbia - 2476 Columbia Rd. – SPA/PUD Resubmittal

On June 17, 2024 the above referenced project was presented to the City Council and the Final PUD plans were approved. Since then, the development team has brought on Krieger Klatt Architects, Inc. (KKA) to be the Architect of Record for the project. We have redesigned the project to better align with today's market and construction costs while still maintaining the original intent from the approved PUD. We have updated all plans and are including them in this resubmittal package titled "02.13.2025 Revised PUD". We are seeking approval from the Planning Commission for the attached plans to then meet with City Council to finalize the PUD and revised Development Agreement. Please see below for a list of notable revisions that were made to the original PUD plans.

1. Eliminated the Southern Building:
 - a. Building 2 separate buildings is an expensive endeavor. We are now proposing one 4-story building on the larger North Parcel between Columbia and Cambridge.
 - b. The Southern parcel now contains a surface parking lot with carports (sheet **A.202**) and a pocket park on Coolidge. The pocket park contains seating areas at the Cambridge/Coolidge corner with a large landscaping area to screen the parking area from Coolidge and the neighbors to the east.
2. Building Revisions:
 - a. Per ordinance, the original PUD was using sloped walls on floor 4 to be classified as a "mansard roof" which allows the building height to be measured to the midpoint of the sloped "roof". This measured 40'-0" to the midpoint but the flat "deck" of the roof would have been 46'-0". The proposed building eliminated these sloped walls. The measurement to the top of deck is 46'-0" so, the building massing has not changed from the original PUD plans.
 - i. Please see sheet **A.400** "Building Section 1" which contains an outline of the original PUD building. Note that the roof deck in the original plans was at 45'-0". This does not include the roof pitch. In our opinion, the original building would have had a roof deck height of 46'-0" which matches what we are proposing.
 - ii. The lower parapet is set at 47'-0" (1' above the roof deck) to allow for proper waterproofing.
 - iii. The upper parapet (located at the North and South corner of the building) is set at 50'-0" and is being proposed to provide visual interest at these premier corners. Please refer to sheets **A.103, A.200, and A.201**.
 - b. The first-floor lobby has been increased in size to allow for all amenities and mail room to be located on this floor. The increased lobby width will be more attractive and will create more pedestrian activity along Coolidge. The stairs and elevators have been relocated to allow for living spaces to be on the corners of the building.
 - c. The unit count has decreased from 57 units to 51 units with a similar mix of 1-bedroom and studio units.
 - d. Inset and wall-hung balconies are being proposed for most units. The 4th floor has no wall-hung balconies facing the neighbors to the east.
 - e. The exterior materials being proposed are brick/masonry and standing seam metal siding facing Coolidge. This matches the original design intent while adjusting for the new building look. We are proposing a red/brown brick instead of the white/cream brick from the original PUD as we feel the new brick color will fit in better with the surrounding properties and look of Berkley.

- f. The exterior materials being proposed on the East, part of the North, and part of the South elevations are a mixture of brick/masonry and cement board siding. We feel that the horizontal siding on these elevations helps the transition to the single-family residential lots to the east.
- 3. Important items to point out that remain the same or less than the original PUD drawings.
 - a. Eliminated the southern building
 - b. Reduced the unit count from 57 units to 51 units
 - c. Parking ratio increases from 1.351 to 1.353 and does not contain any tandem parking spaces
 - d. Compact parking ratio is 34.8% of the total parking provided, which is under the 36% that was approved in the original PUD.
 - e. The N/S drive aisle in the covered parking area is now a two-way drive to aid in vehicular circulation.
 - f. Glazing requirements are being met
 - g. The first floor parking area has openings with screening to hide the parking spaces but also create an attractive elevation.
 - h. The North setback remains 10'. The South setback remains 0'. The West (Coolidge) setback has decreased from 5'-3 3/4" to 2'-0". The East setback has increased from 31'-10" to 33'-7 1/8".
 - i. Please note that the benches, trash cans, landscaping and sidewalk improvements on the Coolidge frontage remains from the original PUD.
 - ii. Please note that the original PUD plans had exterior balconies on Coolidge that were 2'-0 1/8" from the West property line. We are proposing inset balconies on this façade which will be match this 2'-0" setback from the property line.
 - i. The required Type A (accessible) dwelling units will be provided as required per code.
 - j. The required 6' masonry screen wall between the property and the neighboring properties to the east will remain.
 - i. Please note that the Southern parcel parking is pushed 5' west of the property line to allow for plantings between the screen wall and the parking spaces.

We believe that the revisions that are being proposed meet the design intent of the original approved PUD plans, while providing some enhancements that will create a beautiful development. We are very excited to present this to you and look forward to your feedback and approval to proceed. If you have any further comments, concerns or questions; please do not hesitate to contact me. We look forward to your next review and bringing this project to fruition.

Thank you,

Raymond J. Phillips
Director of Design
ray@kriegerklatt.com
248-414-9270 ext. 106



Memorandum

To: Joseph K. Anderson, PE, PLA, LEED-AP Date: October 24, 2023

From: Mohamed Aguib, PE Project: The Columbia
: Traffic Engineer

RE: The Columbia Traffic Project Number: 20388.00
Impact Assessment Memo

This traffic impact assessment memorandum was prepared for the proposed 57-unit residential development in Berkley. The project site is adjacent to Coolidge Highway between Cambridge Road and Columbia Road and covering four residential lots with existing single-family houses. The proposed development consists of redeveloping the subject lots into a 57-unit 3.5 story apartment building with parking lot on the ground level. Site access is currently provided on both Cambridge Road and Columbia Road and is intended to be maintained similarly. An overview of the study area and areas of interest are shown in **Figure 1**.

The purpose of this traffic impact assessment memo is to estimate trip generated numbers for the proposed residential use and to compare them with those generated by the code compliant uses previously approved for the lots but not built. In addition, the study discusses the results of capacity analysis during morning and evening peak hours at the nearest intersections based on historical data from MDOT and traffic assumptions. The conclusions of this study are summarized below:

Conclusions

- ITE trip generation rates and estimates were utilized to calculate the number of weekday morning (AM) and evening (PM) peak hour peak hour trips. The latest ITE Trip Generation Manual 11th Edition was utilized accordingly as shown on Table 1.
- The proposed development, which includes residential only, is expected to generate 41 trips (10 enter and 31 exit) during the morning (AM) and 45 trips (28 enter and 17 exit) during the evening peak hours.
- The code-compliant uses, which includes retail, office and residential, for the same lots would generate 52 trips (22 enter and 30 exit) during the morning (AM) and 86 trips (44 enter and 42 exit) during the evening peak hours.
- The proposed development is expected to generate lower trips than those under the code-compliant plan, consequently, less impact on the surrounding roadway network.
- The subject site is planned to maintain the existing full-access driveways on Cambridge Road and Columbia Road. All driveways are stop-controlled on the minor approach.

- Surface parking is provided on site on the ground level of the building.
- Pedestrian walking facilities and accessibility will be maintained around the site and connecting to the neighborhood.
- At the intersection of Coolidge Highway and Cambridge Road, the results of the morning (AM) and evening (PM) peak hour capacity analysis for the existing and future conditions show acceptable levels of service.
- At the intersection of Coolidge Highway and Columbia Road, the results of the morning (AM) and evening (PM) capacity analysis for the existing and future conditions show lower than acceptable levels of service at the minor approaches, side street approaches. However, the levels of service for the major approaches are acceptable. The lower levels of service during the evening (PM) peak hour are mainly due to the high traffic volume on Coolidge Highway, which is typical for most driveways along this major street.

Roadway Network Characteristics

Coolidge Highway is a three-lane minor arterial with a posted speed limit of 30 mph in the study area. The cross section of Coolidge Highway consists of one-lane each way and a center two-way left-turn lane. Bike lanes and pedestrian sidewalks are provided on both sides of the road. A traffic count station located south of the site shows an AADT (2022) of 20,239 as per Michigan Department of Transportation's (MDOT) Transportation Data Management System (TDMS).

Traffic Data from MDOT

Turning movement counts in the study area, for limited movements, were collected from MDOT database and used accordingly to estimate the traffic volumes at the intersections of Coolidge Highway at Cambridge Road and Columbia Road. These traffic counts were conducted in October of 2022 at a station south of Eleven Mile Road, for the major approaches on Coolidge Highway, during a weekday morning (7:00am to 9:00am) and evening (4:00pm to 6:00pm) peak periods. The existing peak hour traffic volumes are shown in Figure 2.

Trip Generation Summary

The published peak hour trip generation rates, along with inbound/outbound distribution from the Institute of Transportation Engineers (ITE) Trip Generation Manual (11th Edition) were utilized to calculate the number off peak hour trips for the proposed and code-compliant uses.

For the proposed Berkley Condos development, the land use in this evaluation includes Multifamily Housing (Low-rise). The proposed development is expected to generate a total of 41 trips (10 enter and 31 out) during the morning peak hour and a total of 45 trips (28 enter and 17 exit) during the evening peak hour, as shown in Table 1 and Figure 4. The table also shows the uses approved for the subject lots and their relative sizes which include retail, office, and multifamily uses.

The comparison of the proposed (residential only) use and the code-compliant uses have shown that the proposed uses would generate a lower number of trips during the morning and evening peak hours. This difference in the total number of trips is shown in the last line of Table 1.

Table 1: Trip Generation Summary¹

Type	Use	ITE Code	Size	Units	Weekday Trips	AM Peak-Hour Trips			PM Peak-Hour Trips		
						In	Out	Total	In	Out	Total
Proposed	Multifamily Housing (Low-Rise)	220	57	DU	441	10	31	41	28	17	45
	Total Trips				441	10	31	41	28	17	45
Code Compliant	Retail	822	5,760	SF	473	12	8	20	26	27	53
	Office	712	3,150	SF	45	4	1	5	2	5	7
	Multifamily Housing (Low-Rise)	220	12	DU	152	6	21	27	16	10	26
	Total Trips				670	22	30	52	44	42	86
Difference between Proposed and Code Compliant Uses					-229	-12	1	-11	-16	-25	-41

¹A trip is a one-directional vehicular movement into or out of the site. These forecasts are based on trip rates and application methodology recommended by the Institute of Transportation Engineers in its *Trip Generation Manual –11th Edition* (2021).

Capacity Analysis for Existing and Future Conditions

Method and Criteria – Intersection capacity analysis was conducted using the Synchro 11 computerized traffic model, based on methodologies contained in the Transportation Research Board's Highway Capacity Manual (HCM).

The primary objective of the capacity analysis is to determine the level of service, a qualitative measure of the “ease” of traffic flow based on vehicular delay. Analytical models are used to estimate the average control delay by approach and vehicular (through or turning) movement – and in the case of signalized and all-way stop-controlled intersections – the overall intersection as well. The traffic models account for lane configuration, grade (if any), type of traffic control, traffic volume and composition, and other traffic flow parameters.

Level of service (LOS) is expressed on a letter-based grading scale, with A being the highest level and F being the lowest level. Historically, achieving a LOS D or better has been the normal objective in an urban or suburban area; however, LOS E or worse may be unavoidable at some locations along heavily traveled roadways.

During the morning (AM) and evening (PM) peak hours, the results of the capacity analysis for the existing and future conditions show acceptable levels of service of D or better at the intersection of Coolidge Highway at Cambridge Road.

During the morning (AM) and evening (PM) peak hours, the results of the capacity analysis for the existing and future conditions show lower than acceptable levels of service for the minor approaches at the intersection of Coolidge Highway at Columbia Road. However, the levels of service for the major approaches are acceptable. These levels were found during existing

conditions and are expected to continue in the future after the proposed development completion. However, this conditions are typical of most of the minor approaches along this corridor and are expected to operate normally.

Conclusion

The proposed residential development is expected to maintain the expected addition of traffic impacts on the study area. The results of additional site generated traffic are expected to be within roadway and intersection capacities. In comparison with the code compliant site plan, the proposed development is reducing the number of added vehicles to the roadway network.

APPENDIX

1. Traffic Study Figures
2. Site Plans (Proposed & Code Compliant)
3. SYNCHRO Analysis Output Sheets
4. MDOT TDMS Traffic Count Data

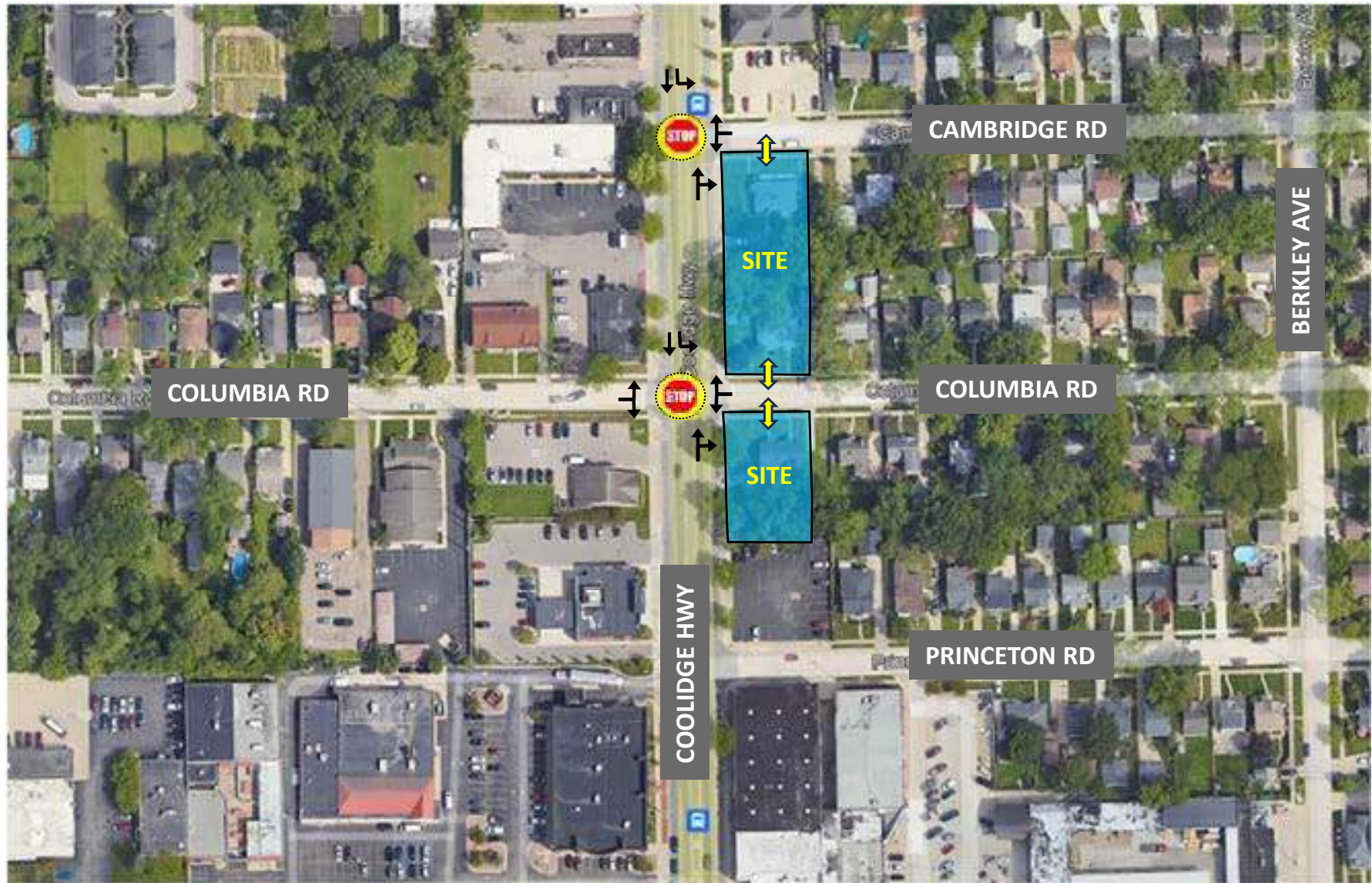


Figure 1. Study Area

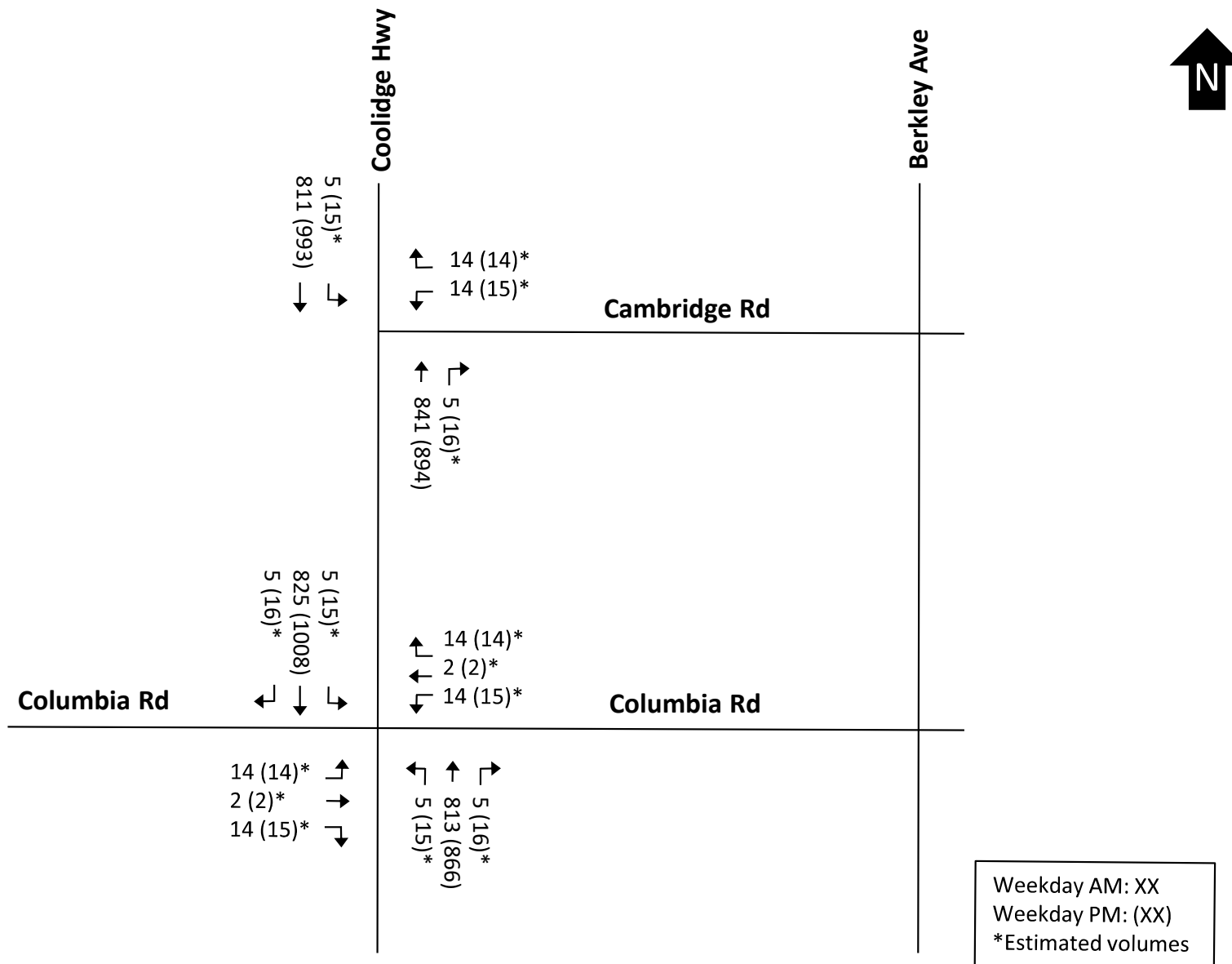


Figure 2. Existing Peak-Hour Volumes

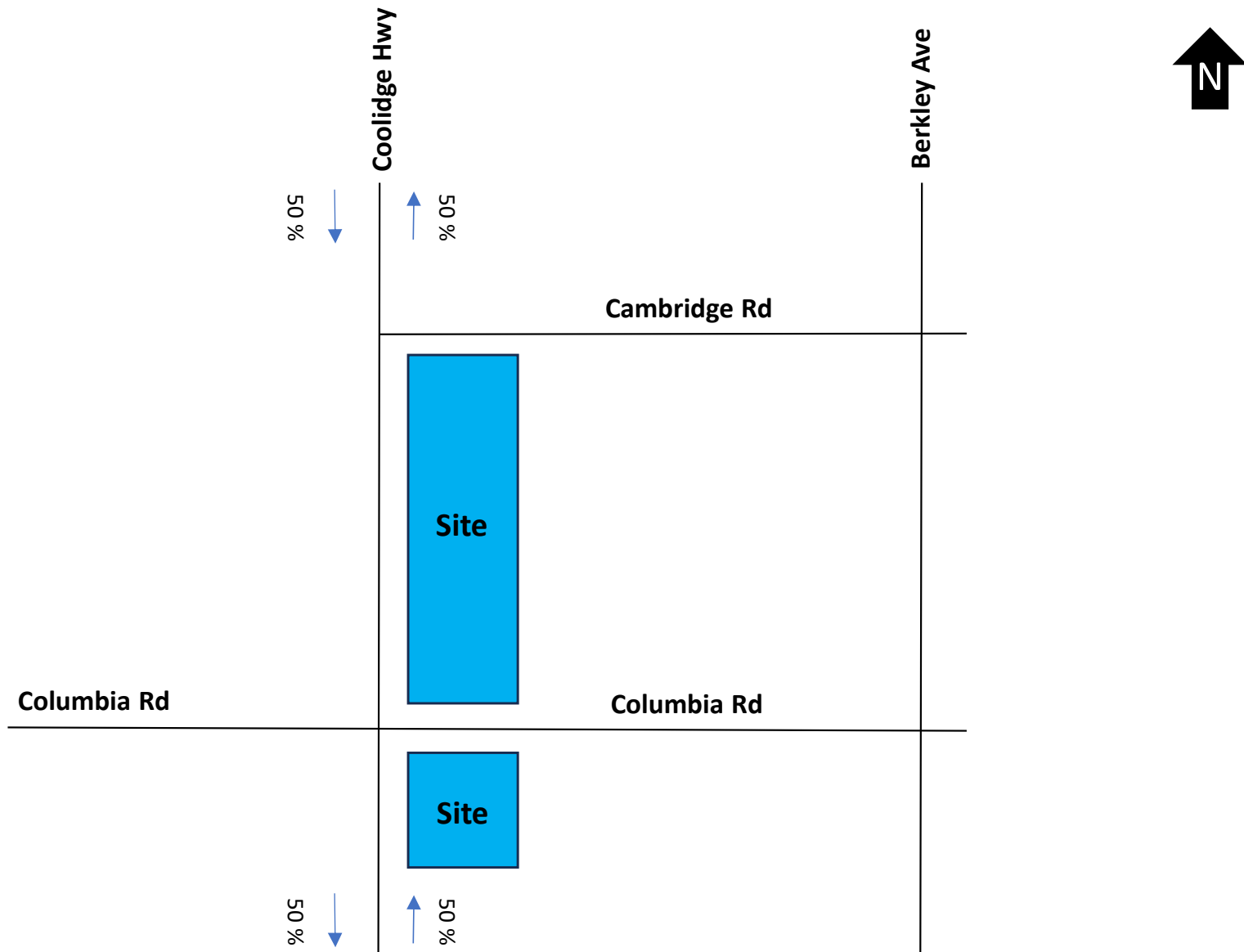


Figure 3. Trip Distribution

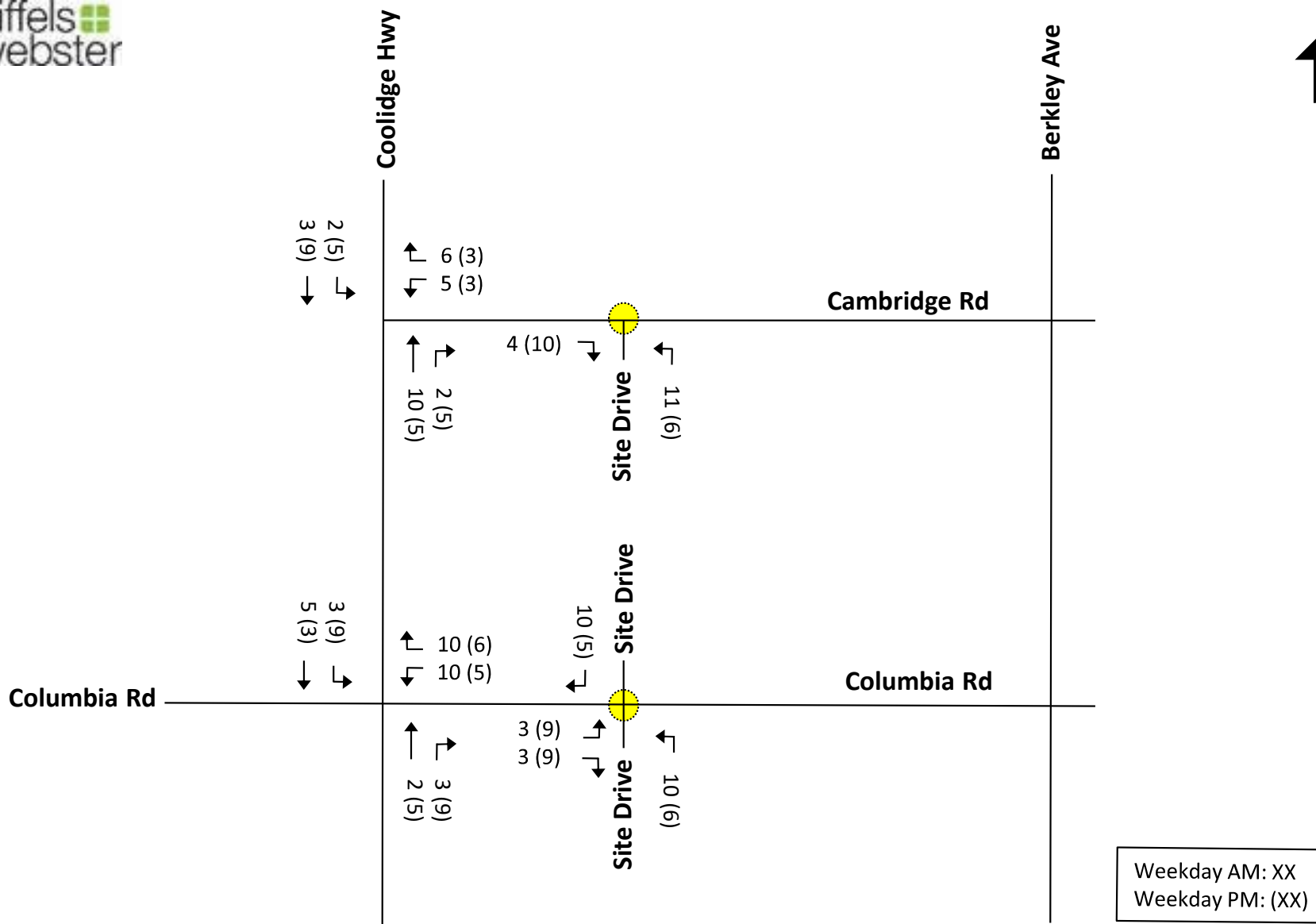


Figure 4. Site Generated Peak-Hour Trip Assignment

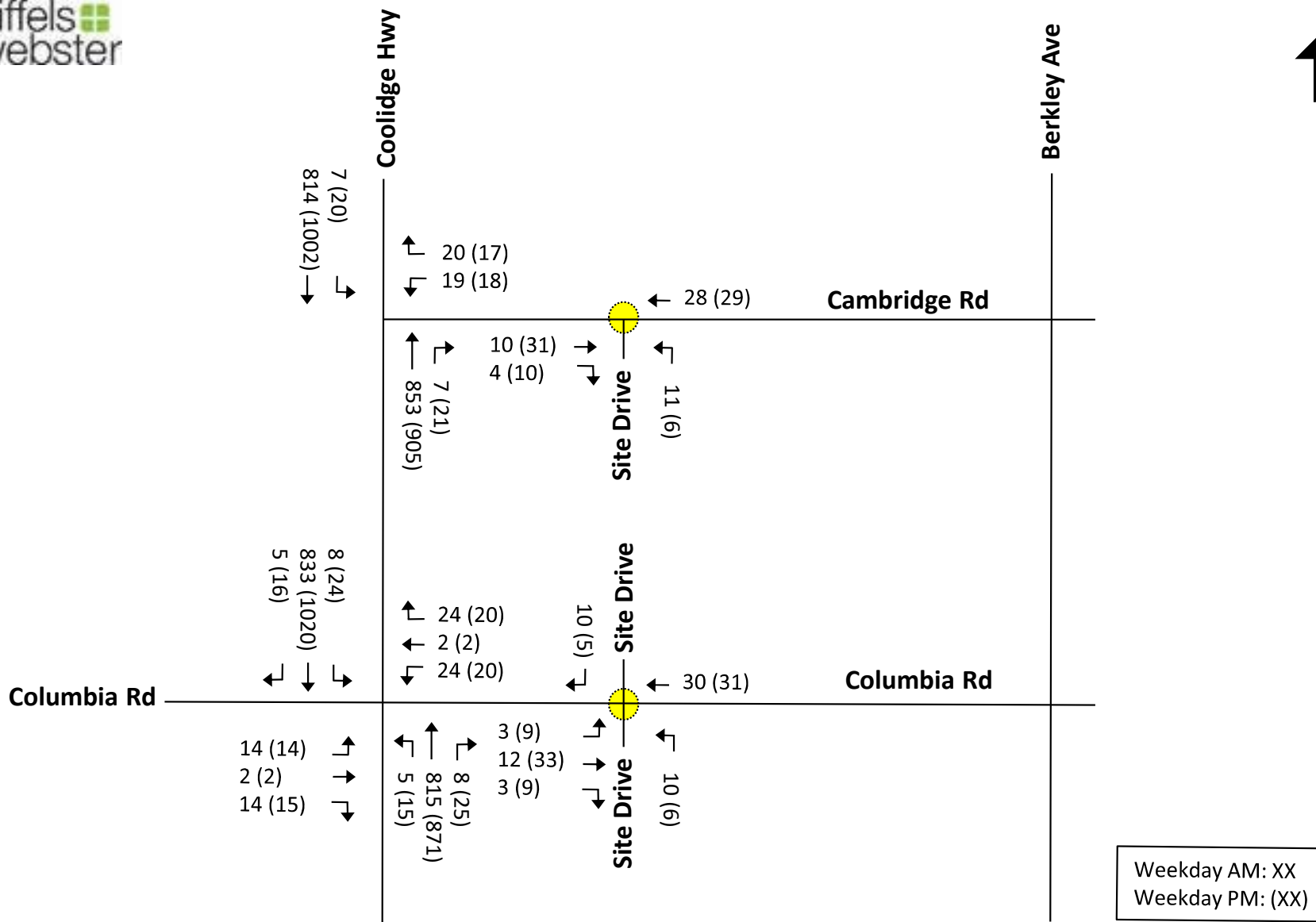
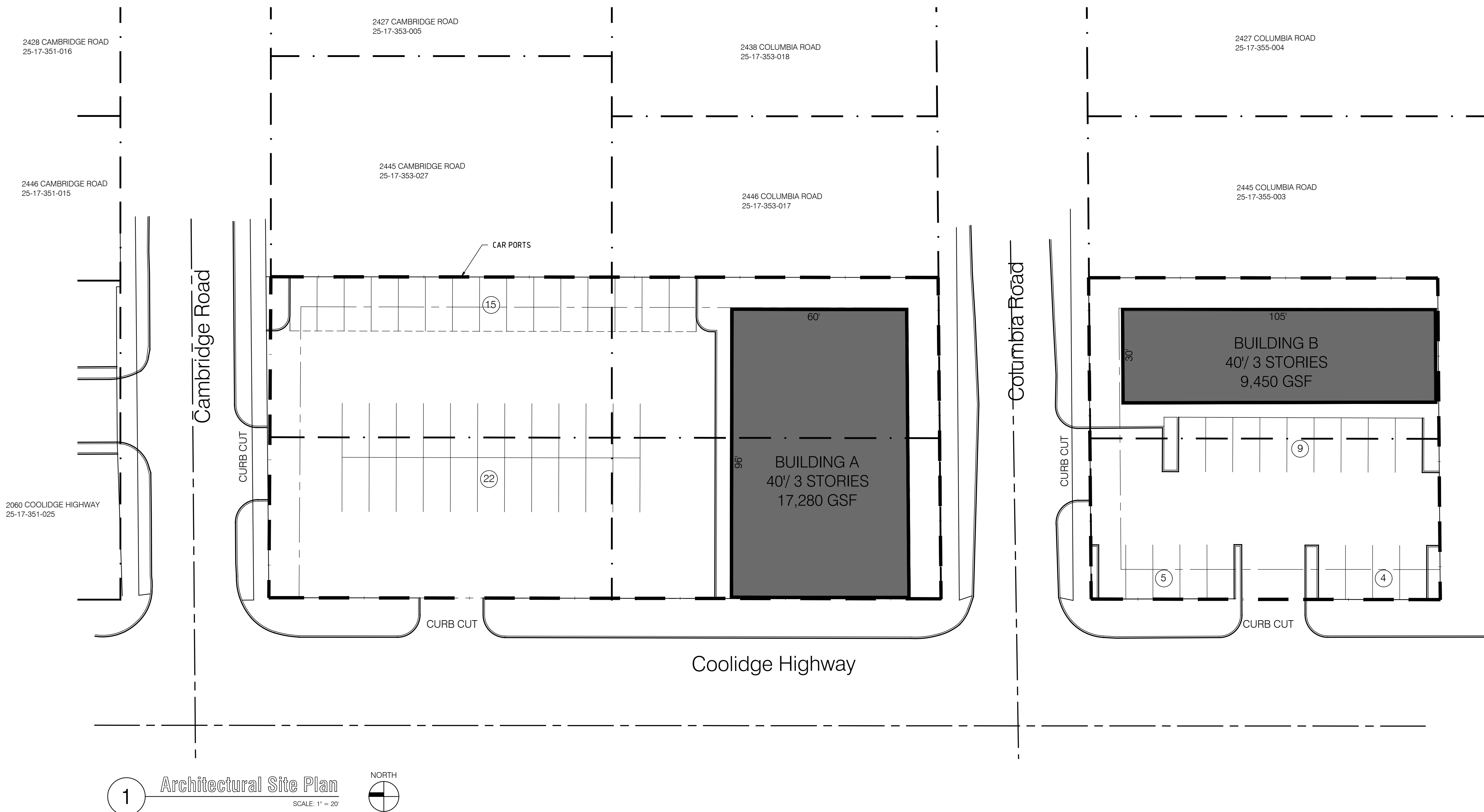


Figure 5. Future Peak-Hour Volumes



BUILDING A

Zoned: Gateway		
Building Use:	Floor 1	Floor 2-3
	Retail/ Office	Multifamily
	5,760 SF	11,520 SF

Unit Count:	Floor 2 4 Units	Floor 3 4 Units
-------------	--------------------	--------------------

Maximum Building Height:
40' Max Height - 40' Tall

Maximum Lot Coverage:
90%

Setbacks:	
Required	Proposed
Front: 10'	10'
Sides: 0'	0'
Rear: 10'	10'

Parking

<p>Retail/ Office: One space per 225 SF of usable floor area</p>
--

Multi-family: Two spaces per dwelling unit

Retail/ Office:	18 spaces required
Multi-family:	16 spaces required
Total:	34 spaces required
37 Spaces Provided	

BUILDING B

Zoned: Gateway		
Building Use:	Floor 1	Floor 2-3
	Retail/ Office	Multifamily
	3,150 SF	6,300 SF

Unit Count:	Floor 2	Floor 3
	2 Units	2 Units

Maximum Building Height:
40' Max Height - 40' Tall

Maximum Lot Coverage:
90%





Setbacks:	
Required	Proposed
Front: 10'	10'
Sides: 0'	0'
Rear: 10'	10'

Parking

Retail/ Office: One space per 225 SF of usable floor area

Multi-family: Two spaces per dwelling unit

Retail/ Office:	10 spaces required
Multi-family:	8 spaces required
Total:	18 spaces required
18 Spaces Provided	




Intersection						
Int Delay, s/veh	0.4					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	14	14	841	5	5	811
Future Vol, veh/h	14	14	841	5	5	811
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	100	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	15	15	914	5	5	882
Major/Minor	Minor1	Major1	Major2			
Conflicting Flow All	1809	917	0	0	919	0
Stage 1	917	-	-	-	-	-
Stage 2	892	-	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218	-
Pot Cap-1 Maneuver	87	330	-	-	743	-
Stage 1	390	-	-	-	-	-
Stage 2	400	-	-	-	-	-
Platoon blocked, %			-	-		-
Mov Cap-1 Maneuver	86	330	-	-	743	-
Mov Cap-2 Maneuver	219	-	-	-	-	-
Stage 1	390	-	-	-	-	-
Stage 2	397	-	-	-	-	-
Approach	WB	NB	SB			
HCM Control Delay, s	20.5	0	0.1			
HCM LOS	C					
Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT		
Capacity (veh/h)	-	-	263	743	-	
HCM Lane V/C Ratio	-	-	0.116	0.007	-	
HCM Control Delay (s)	-	-	20.5	9.9	-	
HCM Lane LOS	-	-	C	A	-	
HCM 95th %tile Q(veh)	-	-	0.4	0	-	

Intersection												
Int Delay, s/veh	2											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↔			↔		↖	↗		↖	↗	
Traffic Vol, veh/h	14	2	14	14	2	14	5	813	5	5	825	5
Future Vol, veh/h	14	2	14	14	2	14	5	813	5	5	825	5
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	100	-	-	100	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	95	95	95	95	95	95	95	95	95	95	95	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	15	2	15	15	2	15	5	856	5	5	868	5

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	1758	1752	871	1758	1752	859	873	0	0	861	0	0
Stage 1	881	881	-	869	869	-	-	-	-	-	-	-
Stage 2	877	871	-	889	883	-	-	-	-	-	-	-
Critical Hdwy	7.12	6.52	6.22	7.12	6.52	6.22	4.12	-	-	4.12	-	-
Critical Hdwy Stg 1	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.518	4.018	3.318	3.518	4.018	3.318	2.218	-	-	2.218	-	-
Pot Cap-1 Maneuver	66	85	350	66	85	356	773	-	-	781	-	-
Stage 1	341	365	-	347	369	-	-	-	-	-	-	-
Stage 2	343	368	-	338	364	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	61	84	350	61	84	356	773	-	-	781	-	-
Mov Cap-2 Maneuver	61	84	-	61	84	-	-	-	-	-	-	-
Stage 1	339	363	-	345	367	-	-	-	-	-	-	-
Stage 2	325	366	-	320	362	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB	
HCM Control Delay, s	55.4		54.7		0.1		0.1	
HCM LOS	F		F					

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	773	-	-	102 103	781	-	-
HCM Lane V/C Ratio	0.007	-	-	0.31 0.307	0.007	-	-
HCM Control Delay (s)	9.7	-	-	55.4 54.7	9.6	-	-
HCM Lane LOS	A	-	-	F F	A	-	-
HCM 95th %tile Q(veh)	0	-	-	1.2 1.2	0	-	-





Intersection						
Int Delay, s/veh	0					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Vol, veh/h	0	0	0	0	0	0
Future Vol, veh/h	0	0	0	0	0	0
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	0	0	0	0	0

Major/Minor	Major1	Major2	Minor1		
Conflicting Flow All	0	0	1	0	2
Stage 1	-	-	-	-	1
Stage 2	-	-	-	-	1
Critical Hdwy	-	-	4.12	-	6.42
Critical Hdwy Stg 1	-	-	-	-	5.42
Critical Hdwy Stg 2	-	-	-	-	5.42
Follow-up Hdwy	-	-	2.218	-	3.518
Pot Cap-1 Maneuver	-	-	1622	-	1021
Stage 1	-	-	-	-	1022
Stage 2	-	-	-	-	1022
Platoon blocked, %	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	1622	-	1021
Mov Cap-2 Maneuver	-	-	-	-	1021
Stage 1	-	-	-	-	1022
Stage 2	-	-	-	-	1022

Approach	EB	WB	NB
HCM Control Delay, s	0	0	0
HCM LOS			A

Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT
Capacity (veh/h)	-	-	-	1622	-
HCM Lane V/C Ratio	-	-	-	-	-
HCM Control Delay (s)	0	-	-	0	-
HCM Lane LOS	A	-	-	A	-
HCM 95th %tile Q(veh)	-	-	-	0	-

Intersection												
Int Delay, s/veh	0											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		<div>↕</div>			<div>↕</div>			<div>↕</div>			<div>↕</div>	
Traffic Vol, veh/h	0	0	0	0	0	0	0	0	0	0	0	0
Future Vol, veh/h	0	0	0	0	0	0	0	0	0	0	0	0
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	0	0	0	0	0	0	0	0	0	0	0	0
Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	1	0	0	1	0	0	2	2	1	2	2	1
Stage 1	-	-	-	-	-	-	1	1	-	1	1	-
Stage 2	-	-	-	-	-	-	1	1	-	1	1	-
Critical Hdwy	4.12	-	-	4.12	-	-	7.12	6.52	6.22	7.12	6.52	6.22
Critical Hdwy Stg 1	-	-	-	-	-	-	6.12	5.52	-	6.12	5.52	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.12	5.52	-	6.12	5.52	-
Follow-up Hdwy	2.218	-	-	2.218	-	-	3.518	4.018	3.318	3.518	4.018	3.318
Pot Cap-1 Maneuver	1622	-	-	1622	-	-	1020	894	1084	1020	894	1084
Stage 1	-	-	-	-	-	-	1022	895	-	1022	895	-
Stage 2	-	-	-	-	-	-	1022	895	-	1022	895	-
Platoon blocked, %		-	-		-	-						
Mov Cap-1 Maneuver	1622	-	-	1622	-	-	1020	894	1084	1020	894	1084
Mov Cap-2 Maneuver	-	-	-	-	-	-	1020	894	-	1020	894	-
Stage 1	-	-	-	-	-	-	1022	895	-	1022	895	-
Stage 2	-	-	-	-	-	-	1022	895	-	1022	895	-
Approach	EB		WB				NB			SB		
HCM Control Delay, s	0		0				0			0		
HCM LOS							A			A		
Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1				
Capacity (veh/h)	-	1622	-	-	1622	-	-	-				
HCM Lane V/C Ratio	-	-	-	-	-	-	-	-				
HCM Control Delay (s)	0	0	-	-	0	-	-	0				
HCM Lane LOS	A	A	-	-	A	-	-	A				
HCM 95th %tile Q(veh)	-	0	-	-	0	-	-	-				

Intersection						
Int Delay, s/veh	0.5					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	15	14	894	16	15	993
Future Vol, veh/h	15	14	894	16	15	993
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	100	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	16	15	972	17	16	1079

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	2092	981	0	0	989
Stage 1	981	-	-	-	-
Stage 2	1111	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218
Pot Cap-1 Maneuver	58	303	-	-	699
Stage 1	363	-	-	-	-
Stage 2	315	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	57	303	-	-	699
Mov Cap-2 Maneuver	178	-	-	-	-
Stage 1	363	-	-	-	-
Stage 2	308	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	23.9	0	0.2
HCM LOS	C		




Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	222	699
HCM Lane V/C Ratio	-	-	0.142	0.023
HCM Control Delay (s)	-	-	23.9	10.3
HCM Lane LOS	-	-	C	B
HCM 95th %tile Q(veh)	-	-	0.5	0.1

Intersection												
Int Delay, s/veh	4.3											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↔			↔		↔	↔		↔	↔	
Traffic Vol, veh/h	14	2	15	14	2	15	15	866	16	15	1008	16
Future Vol, veh/h	14	2	15	14	2	15	15	866	16	15	1008	16
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	100	-	-	100	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	15	2	16	15	2	16	16	941	17	16	1096	17

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	2128	2127	1105	2128	2127	950	1113	0	0	958	0	0
Stage 1	1137	1137	-	982	982	-	-	-	-	-	-	-
Stage 2	991	990	-	1146	1145	-	-	-	-	-	-	-
Critical Hdwy	7.12	6.52	6.22	7.12	6.52	6.22	4.12	-	-	4.12	-	-
Critical Hdwy Stg 1	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.518	4.018	3.318	3.518	4.018	3.318	2.218	-	-	2.218	-	-
Pot Cap-1 Maneuver	36	50	256	36	50	315	627	-	-	718	-	-
Stage 1	245	277	-	300	327	-	-	-	-	-	-	-
Stage 2	296	324	-	242	274	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	32	48	256	31	48	315	627	-	-	718	-	-
Mov Cap-2 Maneuver	32	48	-	31	48	-	-	-	-	-	-	-
Stage 1	239	271	-	292	318	-	-	-	-	-	-	-
Stage 2	272	316	-	220	268	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB	
HCM Control Delay, s	131.3		135.1		0.2		0.1	
HCM LOS	F		F					

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	627	-	-	58	57	718	-
HCM Lane V/C Ratio	0.026	-	-	0.581	0.591	0.023	-
HCM Control Delay (s)	10.9	-	-	131.3	135.1	10.1	-
HCM Lane LOS	B	-	-	F	F	B	-
HCM 95th %tile Q(veh)	0.1	-	-	2.3	2.4	0.1	-





Intersection						
Int Delay, s/veh	0					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Vol, veh/h	31	0	0	29	0	0
Future Vol, veh/h	31	0	0	29	0	0
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	34	0	0	32	0	0
Major/Minor	Major1		Major2		Minor1	
Conflicting Flow All	0	0	34	0	66	34
Stage 1	-	-	-	-	34	-
Stage 2	-	-	-	-	32	-
Critical Hdwy	-	-	4.12	-	6.42	6.22
Critical Hdwy Stg 1	-	-	-	-	5.42	-
Critical Hdwy Stg 2	-	-	-	-	5.42	-
Follow-up Hdwy	-	-	2.218	-	3.518	3.318
Pot Cap-1 Maneuver	-	-	1578	-	939	1039
Stage 1	-	-	-	-	988	-
Stage 2	-	-	-	-	991	-
Platoon blocked, %	-	-		-		
Mov Cap-1 Maneuver	-	-	1578	-	939	1039
Mov Cap-2 Maneuver	-	-	-	-	939	-
Stage 1	-	-	-	-	988	-
Stage 2	-	-	-	-	991	-
Approach	EB		WB		NB	
HCM Control Delay, s	0		0		0	
HCM LOS					A	
Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT	
Capacity (veh/h)	-	-	-	1578	-	
HCM Lane V/C Ratio	-	-	-	-	-	
HCM Control Delay (s)	0	-	-	0	-	
HCM Lane LOS	A	-	-	A	-	
HCM 95th %tile Q(veh)	-	-	-	0	-	

Intersection												
Int Delay, s/veh	0											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Vol, veh/h	0	33	0	0	31	0	0	0	0	0	0	0
Future Vol, veh/h	0	33	0	0	31	0	0	0	0	0	0	0
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	0	36	0	0	34	0	0	0	0	0	0	0

Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	34	0	0	36	0	0	70	70	36	70	70	34
Stage 1	-	-	-	-	-	-	36	36	-	34	34	-
Stage 2	-	-	-	-	-	-	34	34	-	36	36	-
Critical Hdwy	4.12	-	-	4.12	-	-	7.12	6.52	6.22	7.12	6.52	6.22
Critical Hdwy Stg 1	-	-	-	-	-	-	6.12	5.52	-	6.12	5.52	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.12	5.52	-	6.12	5.52	-
Follow-up Hdwy	2.218	-	-	2.218	-	-	3.518	4.018	3.318	3.518	4.018	3.318
Pot Cap-1 Maneuver	1578	-	-	1575	-	-	922	821	1037	922	821	1039
Stage 1	-	-	-	-	-	-	980	865	-	982	867	-
Stage 2	-	-	-	-	-	-	982	867	-	980	865	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	1578	-	-	1575	-	-	922	821	1037	922	821	1039
Mov Cap-2 Maneuver	-	-	-	-	-	-	922	821	-	922	821	-
Stage 1	-	-	-	-	-	-	980	865	-	982	867	-
Stage 2	-	-	-	-	-	-	982	867	-	980	865	-

Approach	EB	WB	NB	SB
HCM Control Delay, s	0	0	0	0
HCM LOS			A	A

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1
Capacity (veh/h)	-	1578	-	-	1575	-	-	-
HCM Lane V/C Ratio	-	-	-	-	-	-	-	-
HCM Control Delay (s)	0	0	-	-	0	-	-	0
HCM Lane LOS	A	A	-	-	A	-	-	A
HCM 95th %tile Q(veh)	-	0	-	-	0	-	-	-

Intersection						
Int Delay, s/veh	0.5					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	19	20	853	7	7	814
Future Vol, veh/h	19	20	853	7	7	814
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	100	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	21	22	927	8	8	885

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	1832	931	0	0	935
Stage 1	931	-	-	-	-
Stage 2	901	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218
Pot Cap-1 Maneuver	84	324	-	-	732
Stage 1	384	-	-	-	-
Stage 2	396	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	83	324	-	-	732
Mov Cap-2 Maneuver	215	-	-	-	-
Stage 1	384	-	-	-	-
Stage 2	392	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	21.5	0	0.1
HCM LOS	C		




Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	260	732
HCM Lane V/C Ratio	-	-	0.163	0.01
HCM Control Delay (s)	-	-	21.5	10
HCM Lane LOS	-	-	C	A
HCM 95th %tile Q(veh)	-	-	0.6	0





Intersection												
Int Delay, s/veh	3.2											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕		↕	↕		↕	↕	
Traffic Vol, veh/h	14	2	14	24	2	24	5	815	8	8	833	5
Future Vol, veh/h	14	2	14	24	2	24	5	815	8	8	833	5
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	100	-	-	100	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	95	95	95	95	95	95	95	95	95	95	95	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	15	2	15	25	2	25	5	858	8	8	877	5





Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	1782	1772	880	1776	1770	862	882	0	0	866	0	0
Stage 1	896	896	-	872	872	-	-	-	-	-	-	-
Stage 2	886	876	-	904	898	-	-	-	-	-	-	-
Critical Hdwy	7.12	6.52	6.22	7.12	6.52	6.22	4.12	-	-	4.12	-	-
Critical Hdwy Stg 1	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.518	4.018	3.318	3.518	4.018	3.318	2.218	-	-	2.218	-	-
Pot Cap-1 Maneuver	64	83	346	64	83	355	767	-	-	777	-	-
Stage 1	335	359	-	345	368	-	-	-	-	-	-	-
Stage 2	339	367	-	331	358	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	58	82	346	59	82	355	767	-	-	777	-	-
Mov Cap-2 Maneuver	58	82	-	59	82	-	-	-	-	-	-	-
Stage 1	333	355	-	343	365	-	-	-	-	-	-	-
Stage 2	311	364	-	312	354	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB	
HCM Control Delay, s	58.3		75.4		0.1		0.1	
HCM LOS	F		F					

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	767	-	-	98	100	777	-
HCM Lane V/C Ratio	0.007	-	-	0.322	0.526	0.011	-
HCM Control Delay (s)	9.7	-	-	58.3	75.4	9.7	-
HCM Lane LOS	A	-	-	F	F	A	-
HCM 95th %tile Q(veh)	0	-	-	1.2	2.4	0	-

Intersection						
Int Delay, s/veh	1.8					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Vol, veh/h	10	4	0	28	11	0
Future Vol, veh/h	10	4	0	28	11	0
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	11	4	0	30	12	0
Major/Minor	Major1		Major2		Minor1	
Conflicting Flow All	0	0	15	0	43	13
Stage 1	-	-	-	-	13	-
Stage 2	-	-	-	-	30	-
Critical Hdwy	-	-	4.12	-	6.42	6.22
Critical Hdwy Stg 1	-	-	-	-	5.42	-
Critical Hdwy Stg 2	-	-	-	-	5.42	-
Follow-up Hdwy	-	-	2.218	-	3.518	3.318
Pot Cap-1 Maneuver	-	-	1603	-	968	1067
Stage 1	-	-	-	-	1010	-
Stage 2	-	-	-	-	993	-
Platoon blocked, %	-	-		-		
Mov Cap-1 Maneuver	-	-	1603	-	968	1067
Mov Cap-2 Maneuver	-	-	-	-	968	-
Stage 1	-	-	-	-	1010	-
Stage 2	-	-	-	-	993	-
Approach	EB		WB		NB	
HCM Control Delay, s	0		0		8.8	
HCM LOS					A	
Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT	
Capacity (veh/h)	968	-	-	1603	-	
HCM Lane V/C Ratio	0.012	-	-	-	-	
HCM Control Delay (s)	8.8	-	-	0	-	
HCM Lane LOS	A	-	-	A	-	
HCM 95th %tile Q(veh)	0	-	-	0	-	

Intersection												
Int Delay, s/veh	2.9											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Vol, veh/h	3	12	3	0	30	0	10	0	0	0	0	10
Future Vol, veh/h	3	12	3	0	30	0	10	0	0	0	0	10
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	3	13	3	0	33	0	11	0	0	0	0	11
Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	33	0	0	16	0	0	60	54	15	54	55	33
Stage 1	-	-	-	-	-	-	21	21	-	33	33	-
Stage 2	-	-	-	-	-	-	39	33	-	21	22	-
Critical Hdwy	4.12	-	-	4.12	-	-	7.12	6.52	6.22	7.12	6.52	6.22
Critical Hdwy Stg 1	-	-	-	-	-	-	6.12	5.52	-	6.12	5.52	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.12	5.52	-	6.12	5.52	-
Follow-up Hdwy	2.218	-	-	2.218	-	-	3.518	4.018	3.318	3.518	4.018	3.318
Pot Cap-1 Maneuver	1579	-	-	1602	-	-	936	837	1065	944	836	1041
Stage 1	-	-	-	-	-	-	998	878	-	983	868	-
Stage 2	-	-	-	-	-	-	976	868	-	998	877	-
Platoon blocked, %		-	-		-	-						
Mov Cap-1 Maneuver	1579	-	-	1602	-	-	925	835	1065	942	834	1041
Mov Cap-2 Maneuver	-	-	-	-	-	-	925	835	-	942	834	-
Stage 1	-	-	-	-	-	-	996	876	-	981	868	-
Stage 2	-	-	-	-	-	-	966	868	-	996	875	-
Approach	EB			WB			NB			SB		
HCM Control Delay, s	1.2			0			8.9			8.5		
HCM LOS							A			A		
Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1				
Capacity (veh/h)	925	1579	-	-	1602	-	-	1041				
HCM Lane V/C Ratio	0.012	0.002	-	-	-	-	-	0.01				
HCM Control Delay (s)	8.9	7.3	0	-	0	-	-	8.5				
HCM Lane LOS	A	A	A	-	A	-	-	A				
HCM 95th %tile Q(veh)	0	0	-	-	0	-	-	0				




Intersection						
Int Delay, s/veh	0.5					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	18	17	905	21	20	1002
Future Vol, veh/h	18	17	905	21	20	1002
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	100	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	20	18	984	23	22	1089
Major/Minor	Minor1	Major1	Major2			
Conflicting Flow All	2129	996	0	0	1007	0
Stage 1	996	-	-	-	-	-
Stage 2	1133	-	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218	-
Pot Cap-1 Maneuver	55	297	-	-	688	-
Stage 1	357	-	-	-	-	-
Stage 2	307	-	-	-	-	-
Platoon blocked, %			-	-		-
Mov Cap-1 Maneuver	53	297	-	-	688	-
Mov Cap-2 Maneuver	172	-	-	-	-	-
Stage 1	357	-	-	-	-	-
Stage 2	297	-	-	-	-	-
Approach	WB	NB	SB			
HCM Control Delay, s	25.2	0	0.2			
HCM LOS	D					
Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT		
Capacity (veh/h)	-	-	216	688	-	
HCM Lane V/C Ratio	-	-	0.176	0.032	-	
HCM Control Delay (s)	-	-	25.2	10.4	-	
HCM Lane LOS	-	-	D	B	-	
HCM 95th %tile Q(veh)	-	-	0.6	0.1	-	

Intersection												
Int Delay, s/veh	6.9											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↔			↔		↔	↔		↔	↔	
Traffic Vol, veh/h	14	2	15	20	2	20	15	871	25	24	1020	16
Future Vol, veh/h	14	2	15	20	2	20	15	871	25	24	1020	16
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	100	-	-	100	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	15	2	16	22	2	22	16	947	27	26	1109	17

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	2175	2176	1118	2172	2171	961	1126	0	0	974	0	0
Stage 1	1170	1170	-	993	993	-	-	-	-	-	-	-
Stage 2	1005	1006	-	1179	1178	-	-	-	-	-	-	-
Critical Hdwy	7.12	6.52	6.22	7.12	6.52	6.22	4.12	-	-	4.12	-	-
Critical Hdwy Stg 1	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.518	4.018	3.318	3.518	4.018	3.318	2.218	-	-	2.218	-	-
Pot Cap-1 Maneuver	33	46	252	34	47	311	620	-	-	708	-	-
Stage 1	235	267	-	296	323	-	-	-	-	-	-	-
Stage 2	291	319	-	232	265	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	28	43	252	29	44	311	620	-	-	708	-	-
Mov Cap-2 Maneuver	28	43	-	29	44	-	-	-	-	-	-	-
Stage 1	229	257	-	288	315	-	-	-	-	-	-	-
Stage 2	262	311	-	207	255	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB	
HCM Control Delay, s	163		207		0.2		0.2	
HCM LOS	F		F					

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	620	-	-	51 53	708	-	-
HCM Lane V/C Ratio	0.026	-	-	0.661 0.861	0.037	-	-
HCM Control Delay (s)	11	-	-	163 207	10.3	-	-
HCM Lane LOS	B	-	-	F F	B	-	-
HCM 95th %tile Q(veh)	0.1	-	-	2.6 3.7	0.1	-	-

Intersection						
Int Delay, s/veh	0.7					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Vol, veh/h	31	10	0	29	6	0
Future Vol, veh/h	31	10	0	29	6	0
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	34	11	0	32	7	0
Major/Minor	Major1		Major2		Minor1	
Conflicting Flow All	0	0	45	0	72	40
Stage 1	-	-	-	-	40	-
Stage 2	-	-	-	-	32	-
Critical Hdwy	-	-	4.12	-	6.42	6.22
Critical Hdwy Stg 1	-	-	-	-	5.42	-
Critical Hdwy Stg 2	-	-	-	-	5.42	-
Follow-up Hdwy	-	-	2.218	-	3.518	3.318
Pot Cap-1 Maneuver	-	-	1563	-	932	1031
Stage 1	-	-	-	-	982	-
Stage 2	-	-	-	-	991	-
Platoon blocked, %	-	-		-		
Mov Cap-1 Maneuver	-	-	1563	-	932	1031
Mov Cap-2 Maneuver	-	-	-	-	932	-
Stage 1	-	-	-	-	982	-
Stage 2	-	-	-	-	991	-
Approach	EB		WB		NB	
HCM Control Delay, s	0		0		8.9	
HCM LOS					A	
Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT	
Capacity (veh/h)	932	-	-	1563	-	
HCM Lane V/C Ratio	0.007	-	-	-	-	
HCM Control Delay (s)	8.9	-	-	0	-	
HCM Lane LOS	A	-	-	A	-	
HCM 95th %tile Q(veh)	0	-	-	0	-	

Intersection												
Int Delay, s/veh	1.8											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Vol, veh/h	9	33	9	0	31	0	6	0	0	0	0	5
Future Vol, veh/h	9	33	9	0	31	0	6	0	0	0	0	5
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	10	36	10	0	34	0	7	0	0	0	0	5
Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	34	0	0	46	0	0	98	95	41	95	100	34
Stage 1	-	-	-	-	-	-	61	61	-	34	34	-
Stage 2	-	-	-	-	-	-	37	34	-	61	66	-
Critical Hdwy	4.12	-	-	4.12	-	-	7.12	6.52	6.22	7.12	6.52	6.22
Critical Hdwy Stg 1	-	-	-	-	-	-	6.12	5.52	-	6.12	5.52	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.12	5.52	-	6.12	5.52	-
Follow-up Hdwy	2.218	-	-	2.218	-	-	3.518	4.018	3.318	3.518	4.018	3.318
Pot Cap-1 Maneuver	1578	-	-	1562	-	-	884	795	1030	888	790	1039
Stage 1	-	-	-	-	-	-	950	844	-	982	867	-
Stage 2	-	-	-	-	-	-	978	867	-	950	840	-
Platoon blocked, %		-	-		-	-						
Mov Cap-1 Maneuver	1578	-	-	1562	-	-	874	789	1030	884	784	1039
Mov Cap-2 Maneuver	-	-	-	-	-	-	874	789	-	884	784	-
Stage 1	-	-	-	-	-	-	943	838	-	975	867	-
Stage 2	-	-	-	-	-	-	973	867	-	943	834	-
Approach	EB			WB			NB			SB		
HCM Control Delay, s	1.3			0			9.2			8.5		
HCM LOS							A			A		
Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1				
Capacity (veh/h)	874	1578	-	-	1562	-	-	1039				
HCM Lane V/C Ratio	0.007	0.006	-	-	-	-	-	0.005				
HCM Control Delay (s)	9.2	7.3	0	-	0	-	-	8.5				
HCM Lane LOS	A	A	A	-	A	-	-	A				
HCM 95th %tile Q(veh)	0	0	-	-	0	-	-	0				

Record	1	of 1	Goto Record	go
Location ID	63-5139	NPO ID	59455	
Type	SPOT	HPMS ID	1 4 125 040	
On NHS	No	On HPMS	Yes	
LRS ID	0644004	LRS Loc Pl	2 675	
SF Group	Urban Non State	Route Type		
AF Group	NoFactor	Route		
GF Group	Urban Non State	Active	Yes	
Class Dist Grp	NTH 4	Category	Primary	
Seas Class Grp				
WIM Group				
QC Group	Default			
Facct Class	(4) Minor Arterial	Milepost		
Located On	COOLIDGE HWY			
Loc On Alias				
	100 FEET S OF 11 MILE			
More Detail				

Directions: 2-WAY NO SB

AADT								
	Year	AADT	DRV-30	K %	D %	PA	BC	Src
	2022	20,239	1,859	9	53	19,852 (98%)	387 (2%)	
	2021	22,368 ³		9	54	21,318 (95%)	1,050 (5%)	Grown from 2020
	2020	19,031 ³		9	54	18,687 (95%)	944 (5%)	Grown from 2019
	2019	22,987	2,125	9	54	22,554 (98%)	433 (2%)	
	2018	23,242 ³		7	65	22,616 (97%)	626 (3%)	Grown from 2017

VOLUME COUNT				VOLUME TREND	
	Date	Int	Total	Year	Annual Growth
	Wed 10/18/2022	15	20,718	2022	-10%
	Tue 10/18/2022	15	19,769	2021	14%
	Tue 9/15/2019	15	22,987	2020	-15%
	Tue 12/6/2016	60	22,327	2019	-1%



Location Info					
Location ID	63-5139_NB				
County ID	63				
Station ID	9_NB				
Type	I-SECTION				
Functional Class	4				
Located On	COOLIDGE HWY				
	100 FEET S OF 11 MILE				
Direction	NB				
Community	Oak Park				
MPO_ID	58453				
HPMS ID	1_4_125_048				
Agency	Michigan Department of Transportation				
Interval: 15 mins					
Time	15 Min				Hourly Count
	1st	2nd	3rd	4th	
00:00 - 01:00	13	21	6	4	44
01:00 - 02:00	7	5	2	2	16
02:00 - 03:00	4	5	3	5	17
03:00 - 04:00	1	3	4	7	15
04:00 - 05:00	8	7	12	9	36
05:00 - 06:00	18	16	36	33	103
06:00 - 07:00	44	46	67	111	268
07:00 - 08:00	104	125	171	215	615
08:00 - 09:00	245	165	188	167	765
09:00 - 10:00	152	148	142	146	588
10:00 - 11:00	143	133	163	134	573
11:00 - 12:00	153	139	140	161	593
12:00 - 13:00	175	124	171	159	629
13:00 - 14:00	154	169	140	170	633
14:00 - 15:00	139	182	181	194	696
15:00 - 16:00	208	221	234	208	871
16:00 - 17:00	211	224	213	205	853
17:00 - 18:00	244	217	204	201	866
18:00 - 19:00	192	175	187	165	719
19:00 - 20:00	170	124	96	85	475
20:00 - 21:00	86	86	85	57	314
21:00 - 22:00	53	58	43	38	192
22:00 - 23:00	35	40	23	26	124
23:00 - 24:00	28	21	12	12	73
TOTAL					10078

Count Data Info	
Start Date	10/19/2022
End Date	10/20/2022
Start Time	12:00 PM
End Time	12:00 PM
Direction	NB
Notes	
Count Source	63-5139
File Name	63-5139_1003382_10-18-2022.prn
Weather	
Study	
Owner	brownv22
QC Status	Accepted

Location Info					
Location ID	63-5139_SB				
County ID	63				
Station ID	9_SB				
Type	I-SECTION				
Functional Class	4				
Located On	COOLIDGE HWY				
	100 FEET S OF 11 MILE				
Direction	SB				
Community	Oak Park				
MPO_ID	58454				
HPMS ID	1_4_125_048				
Agency	Michigan Department of Transportation				
Interval: 15 mins					
Time	15 Min				Hourly Count
	1st	2nd	3rd	4th	
00:00 - 01:00	15	18	5	8	46
01:00 - 02:00	9	9	8	9	35
02:00 - 03:00	5	4	1	4	14
03:00 - 04:00	2	2	4	8	16
04:00 - 05:00	4	3	7	14	28
05:00 - 06:00	15	13	21	28	77
06:00 - 07:00	39	50	70	74	233
07:00 - 08:00	92	133	167	228	620
08:00 - 09:00	223	200	160	174	757
09:00 - 10:00	117	134	124	154	529
10:00 - 11:00	113	93	115	135	456
11:00 - 12:00	116	133	148	164	561
12:00 - 13:00	157	171	147	156	631
13:00 - 14:00	173	175	160	162	670
14:00 - 15:00	133	143	180	197	653
15:00 - 16:00	215	242	248	276	981
16:00 - 17:00	235	215	217	249	916
17:00 - 18:00	240	240	237	276	993
18:00 - 19:00	251	197	179	163	790
19:00 - 20:00	177	177	144	141	639
20:00 - 21:00	124	137	96	81	438
21:00 - 22:00	93	78	58	63	292
22:00 - 23:00	55	34	43	37	169
23:00 - 24:00	29	23	26	18	96
TOTAL					10640

Count Data Info	
Start Date	10/19/2022
End Date	10/20/2022
Start Time	12:00 PM
End Time	12:00 PM
Direction	SB
Notes	
Count Source	63-5139
File Name	63-5139_1003382_10-18-2022.prn
Weather	
Study	
Owner	brownv22
QC Status	Accepted

**A PROCLAMATION
of the Council of the City of Berkley, Michigan
Honoring the Dedication and Service of John “Jack” Blanchard**

WHEREAS, the safety, security, and preparedness of the City of Berkley are of paramount importance to the well-being of its residents, and;

WHEREAS, after years of service as a Volunteer Firefighter John “Jack” Blanchard has continued to selflessly serve as the City of Berkley’s Emergency Manager on a volunteer basis, dedicating his time, expertise, and unwavering commitment to the development and implementation of emergency response plans, public safety protocols, and disaster preparedness efforts, and;

WHEREAS, through countless hours of service, coordination with regional and state agencies, and dedication to training and emergency exercises, Jack has helped ensure that Berkley is well-equipped to respond to natural and manmade emergencies, protecting lives and minimizing risks, and;

WHEREAS, Jack’s leadership, professionalism, and community spirit exemplify the highest ideals of public service, inspiring confidence and cooperation among state, county and local officials, first responders, and residents alike, and;

WHEREAS, his work during times of potential crisis has been instrumental in maintaining calm, effective, and coordinated responses, and his efforts behind the scenes have made a lasting impact on the City's resilience and preparedness.

NOW, THEREFORE, THE CITY OF BERKLEY HEREBY PROCLAIMS

On behalf of the Berkley City Council and the entire community, the Berkley City Council recognizes and honors Jack Blanchard for his outstanding volunteer service and invaluable contributions as the City’s Emergency Manager.

Proclaimed this 2nd day of June 2025 at a Regular Meeting of the Berkley City Council.

Bridget Dean, Mayor

Attest:

Victoria Mitchell, City Clerk

June 2, 2025 City Council Meeting

Moved by Councilmember _____ and seconded by Councilmember _____ to approve a Charitable Solicitation application submitted by Disarmory Ministries to destroy firearms every Friday and Saturday from 10 a.m.- 2 p.m. now through November 1, 2025 in the parking lot of Greenfield Presbyterian Church, 2312 Greenfield Road. The organization will have a donation bucket placed on the property.

Ayes:

Nays:

Absent:

Motion:

MEMORANDUM

To: Mayor Dean and City Council
From: Victoria Mitchell, City Clerk
Kristen Kapelanski, Community Development Director
Date: *Monday, June 2, 2025*
Subject: Disarmory Ministries Charitable Permit

Madam Mayor and Members of City Council,

Background

- Disarmory Ministries requests approval for a Charitable Solicitation Permit to conduct firearm disposal services. The organization will also have a container on the property for donations.
- The initiative will take place from 10 a.m.-2 p.m. every Friday and Saturday now through November 1st, with the exception of the July 4th weekend, in the parking lot of Greenfield Presbyterian Church, 2312 Greenfield.
- Disarmory Ministries is a non-profit organization offering gift cards in exchange for unwanted firearms, which are destroyed on-site with a miter saw.
- Set up of the operation includes saws on work stands, a pop-up tent, and some associated tables.
- Disarmory Ministries staff will be on site along with two paid firearms experts.
- Disarmory Ministries is currently operating under a promotional event permit at Greenfield Presbyterian Church at 2312 Greenfield. This is valid through June 7, 2025. Each property is permitted up to three promotional events per year.
- The City's code officer has observed the site several times under operation and has reported no issue with noise or traffic. The City has not received any complaints from nearby residences.
- No city services are required.
- All necessary documentation was submitted to the Clerk's Office along with necessary approvals.

Summary

- The Disarmory Ministries Charitable Solicitation Permit Application is before you today for approval.
- Following site visits and a thorough internal review, no issues have been attached to the initiative.
- The application received staff approval.

Recommendation

Approval of a Charitable Solicitation application submitted by Disarmory Ministries to destroy firearms every Friday and Saturday from 10 a.m.- 2 p.m. now through November 1, 2025 in the parking lot of Greenfield Presbyterian Church, 2312 Greenfield Road. The organization will have a donation bucket placed on the property.



City Clerk's Office
3338 Coolidge Hwy.
Berkley, MI 48072
248-658-3300
Fax: 248-658-3301
www.berkleymich.org

CITY OF BERKLEY, MICHIGAN

CHARITABLE SOLICITATION

PERMIT APPLICATION DISPOSITION CHECKLIST

Event Name: FIREARM DISPOSAL

Event Date(s) EVERY FRIDAY AND SATURDAY Location(s) 2312 GREENFIELD Event Hours 10A-2P
MAY 2 - NOV 1

ORGANIZATION:

Organization Name:

DISARMORY MINISTRIES

Headquarters Street Address: 2312 GREENFIELD EVENING DAYS ONLY / PO BOX 721164

City: BERKLEY

State: MI

Zip Code: 48072

Phone: (b) (6)

Website:

DISARMORY.ORG

Tax Exempt Status (as defined by the US Internal Revenue Service):

☒ Non-Profit

☒ 501(c)(3)

☐ N/A

☐ Other (specify) _____

The following documents have been submitted:

Completed application

☒ YES ☐ NO

Valid IRS tax exempt verification

☒ YES ☐ NO

Financial report for the preceding fiscal year

☐ YES ☒ NO *Don't have

Proof of Commercial Liability Coverage

☒ YES ☐ NO

Estimated cost to execute the solicitation / event

☒ YES ☐ NO

Charitable Solicitation / Special Event Hold Harmless Agreement

☒ YES ☐ NO

Application reviewed by:

Public Safety

M. L. 5-20-25

Public Works

A. L. 5-20-25

Planning Official

E. L. 5/24/25

Comments:

Presented to City Council:

Date:

Approved:

YES ☐ NO ☐

Organization Notified

YES ☐ NO ☐

*WE INCORPORATED A FEW MONTHS AGO: WE ARE TOO NEW.



City Clerk's Office
3338 Coolidge Hwy.
Berkley, MI 48072
248-658-3300
Fax: 248-658-3301
www.berkleymich.org

CITY OF BERKLEY, MICHIGAN CHARITABLE SOLICITATION PERMIT APPLICATION

Event Name: FIREARM DISPOSAL

Event Date(s) EVERY FRIDAY AND SATURDAY Location(s) 2312 GREENFIELD Event Hours 10A - 2P
JULY 2 - NOV 1 NOT INCLUDING JULY 4 WEEKEND

ORGANIZATION INFORMATION

Name: DISARMORY MINISTRIES
Street Address: 2312 GREENFIELD ONEVENT DAYS ONLY / PO Box 721164
City: BERKLEY State: MI Zip Code: 48072 Phone: (b) (6)
Website: DISARMORY.ORG

Tax Exempt Status (as defined by the U.S. Internal Revenue Service):

☒ Non-Profit ☒ 501(c)(3) ☐ N/A ☐ Other (specify) _____

CONTACT PERSON (Authorized Representative)

Name CHRIS YAW Title PRESIDENT Phone (b) (6)
Email DISARMORY@GMAIL.COM

AGENTS/OFFICERS OF ORGANIZATION

Name MARY BENSMILLER Title VIC PRESIDENT Phone (b) (6)
Address (b) (6)

Name DR. MIKE OTTO Title TREASURER Phone (b) (6)
Address (b) (6)

Name JESSICA RIENSTRA Title SECRETARY Phone (b) (6)
Address (b) (6)

Describe the type and purpose of the solicitation and how collected funds be used.

BUCKET IS PLACED ON PROPERTY AND PEOPLE ARE INVITED TO GIVE.

The amount of wages, fees, commissions, expenses to be paid to any person in connection with the solicitation, together with the manner in which such wages, fees, expenses, commissions are to be paid:

OUR STAFF OF 2 PAID FIREARMS EXPERTS RECIEVE
\$30/Hr EACH FOR X HRS OF WORK PER WEEK.

Estimated cost of the solicitation

\$580/WEEK X 25 WEEKS = \$14,500

The following documents must be submitted with this application, pursuant to *Chapter 34 Charitable Solicitations, Article I* of the City of Berkley Code of Ordinance:

1. Internal Revenue Service tax exempt documentation for the organization
2. Organization's financial report for the preceding fiscal year
3. Proof of Commercial Liability Coverage on an "occurrence basis" with no less than \$500,000 per occurrence and/or combined single limit, Personal Injury, Bodily Injury, and Property Damage. The insured endorsement portion of the Certificate of Insurance must read EXACTLY as follows:
The City of Berkley, including all elected and appointed officials, all employees, and volunteers, all boards, commissions, and/or authorities and their board members, employees, and volunteers.
4. Charitable Solicitation Hold Harmless Agreement signed by an authorized representative of the organization.

If the above information cannot be furnished, please provide a detailed statement of the reason.

I hereby solemnly swear that no solicitor will be paid, either directly or indirectly, a salary, wage, commission or benefit for his/her service.

 Chris Yau / President 5/13/25
Applicant Signature Print Name/Title Date

For City Use Only

Approved by

Title

Signature

Date



CITY OF BERKLEY MICHIGAN
CHARITABLE SOLICITATION - SPECIAL EVENT
HOLD HARMLESS AGREEMENT

This Hold Harmless Agreement is between DISARMORY MINISTRIES, (the Organization) and the City of Berkley, Michigan, (City) for the date(s) specified below.

Organization Name DISARMORY MINISTRIES
Address PO BOX 721164
City BERKLEY State MI Zip 48076 Phone (b) (6)
Event Name FIREARM DISPOSAL
Event Location 2312 GREENFIELD ROAD Event Date(s) EVERY FRI + SAT
BEGINNING MAY 2 + 3

To the fullest extent permitted by law, the Organization expressly agrees to indemnify and hold harmless the City of Berkley, its elected and appointed officials, its employees and volunteers and others working on behalf of the City, from and against all loss, cost, expense, damage, liability or claims (whether groundless or not) arising out of bodily injury, sickness or disease (including death resulting at any time there from) which may be sustained or claimed by any person or persons participating in the above named event.

This includes damage or destruction of any property (including loss of use) based on any act or omission (negligent or otherwise) of the Organization or anyone acting on its behalf in connection with or incidental to this agreement. The Organization shall, at its own cost and expense, defend any such claim and any suit, action, or proceeding which may be commenced hereunder. In the event of any suit, action or proceeding, the Organization shall pay:

- Any and all judgments which may be recovered.
- Any and all expenses, including, but not limited to, costs, attorneys' fees and settlement expenses which may be incurred.

The Organization shall not be responsible to the City on indemnity for damages caused by or resulting from the City's sole negligence.

Authorized Representative CHRIS YAW Title PRESIDENT
Signature *(Signature)* (Please print) _____ Date 5/13/25



City Clerk's Office
3338 Coolidge Hwy.
Berkley, MI 48072
248-658-3300
Fax: 248-658-3301
www.berkleymich.org

CITY OF BERKLEY, MICHIGAN TEMPORARY TRAFFIC CONTROL ORDER

(TCO NUMBER)

In accordance with the duly adopted Uniform Traffic Code for Cities, Townships, and Villages, the Traffic Engineer hereby issues the following Temporary Traffic Control Order.

Film Permit Applicant: Please provide the information requested in the shaded area. Submit this page with your application.

PROJECT NAME:	FIREARM DISPOSAL
Location	2312 GREENFIELD
DATE/S/TIMES:	MAY 2 - NOV 1 10A - 1P EVERY FRI + SAT *CLOSED TUE & WEDNESDAY
RESPONSIBLE PARTY:	
Name:	CHRIS YAN
Street Address:	2312 GREENFIELD ROAD - DURING EVENT
City/State/ZIP:	BERKLEY, MI 48072
Contact Phone:	(b) (6)
Email address:	DISARMORY @ GMAIL . COM

For Official Use Only:

Action required by City :	
(Attach additional pages if needed)	

Approved by: (Print name and title below)	Signature:	Date:
Public Safety	<i>M. L.</i>	5-20-25
Public Works	<i>Al G.</i>	5-20-25
Planning Official	<i>E. Nis</i>	5.20.25

**Original - Traffic Control File

Submit completed form to City Clerk's Office

January 2017

Date of this notice: 02-28-2025

Employer Identification Number:
33-3714135

Form: 33-4

Number of this notice: CP 575 K

DISARMORY MINISTRIES
6100 W GURREY RD
WAXFORD NC, NC 28391

For assistance you may call us at:
1-800-829-4933

IF YOU WRITE, ATTACH THE
STUB AT THE END OF THIS NOTICE.

WE ASSIGNED YOU AN EMPLOYER IDENTIFICATION NUMBER

Thank you for applying for an Employer Identification Number (EIN). We assigned you EIN 33-3714135. This EIN will identify your entity, accounts, tax returns, tax returns, and documents, even if you have no employees. Please keep this notice in your permanent records.

Taxpayers request an EIN for business and tax purposes. Some taxpayers receive CP575 notices when another person has stolen their identity and are operating using their information. If you did **not** apply for this EIN, please contact us at the phone number or address listed on the top of this notice.

When filing tax documents, making payments, or replying to any related correspondence, it is very important that you use your EIN and complete name and address exactly as shown above. Any variation may cause a delay in processing, result in incorrect information in your account, or even cause you to be assigned more than one EIN. If the information is not correct as shown above, please make the correction using the attached tear-off stub and return it to us.

When you submitted your application for an EIN, you checked the box indicating you are a non-profit organization. Assigning an EIN does not grant tax-exempt status to non-profit organizations. Publication 557, Tax-Exempt Status for Your organization, has details on the application process, as well as information on returns you may need to file. To apply for recognition of tax-exempt status, organizations must complete an application on one of the following forms: Form 1023, Application for Recognition of Exemption Under Section 501(c)(3) of the Internal Revenue Code; Form 1023-EZ, Streamlined Application for Recognition of Exemption Under Section 501(c)(3) of the Internal Revenue Code; Form 1024, Application for Recognition Under Section 501(c)(3) of the Internal Revenue Code; or Form 1024-A, Application for Recognition of Exemption Under Section 501(c)(4) of the Internal Revenue Code.

Nearly all organizations claiming tax-exempt status must file a Form 990-series annual information return (Form 990, 990-EZ, or 990-PF) or notice (Form 990-N) beginning with the year they legally form, even if they have not yet applied for or received recognition of tax-exempt status.

If you become tax-exempt, you will lose tax-exempt status if you fail to file a required return or notice for three consecutive years, unless a filing exception applies to you (search www.irs.gov for Annual Exempt Organization Return: Who Must File). We start calculating this three-year period from the tax year we assigned the EIN to you. If that first tax year isn't a full twelve months, you're still responsible for submitting a return for that year. If you didn't legally form in the same tax year in which you obtained your EIN, contact us at the phone number or address listed at the top of this letter. For the most current information on your filing requirements and other important information, visit www.irs.gov/charities.

IMPORTANT REMINDERS:

- * Keep a copy of this notice in your permanent records. This notice is issued only one time and the IRS will not be able to generate a duplicate copy for you. You may give a copy of this document to anyone asking for proof of your EIN.
- * Use this EIN and your name exactly as they appear at the top of this notice on all your federal tax forms.
- * Refer to this EIN on your tax-related correspondence and documents.
- * Provide future officers of your organization with a copy of this notice.

Your name control associated with this EIN is DISA. You will need to provide this information along with your EIN, if you file your returns electronically.

Safeguard your EIN by referring to Publication 4557, Safeguarding Taxpayer Data: A Guide for Your Business.

You can get any of the forms or publications mentioned in this letter by visiting our website at www.irs.gov/forms-pubs or by calling 800-TAX-FORM (800-829-3676).

If you have questions about your EIN, you can contact us at the phone number or address listed at the top of this notice. If you write, please tear off the stub at the bottom of this notice and include it with your letter.

Thank you for your cooperation.

Keep this part for your records.

CP 575 E (Rev. 7-2007)

Return this part with any correspondence so we may identify your account. Please correct any errors in your name or address.

CP 575 E

9999999999

Your Telephone Number Best Time to Call
() - _____

DATE OF THIS NOTICE: 02-28-2025
EMPLOYER IDENTIFICATION NUMBER: 33-3714138
FORM: SS-4 NOBOD

INTERNAL REVENUE SERVICE
CINCINNATI OH 45999-0023
[Barcode]

DIABLO MINISTRIES
6180 N SURREY RD
BLOOMFIELD HILLS, MI 48301

CERTIFICATE OF INSURANCE

DATE ISSUED/REV
05/15/2025

PRODUCER AND THE NAMED INSURED
Evolution Insurance Brokers, LLC

8722 S. Harrison St
Sandy, UT 84070
(801) 304-5500

INSURED
Divanory Ministries
DBA
2342 Greendale Rd
Berkeley, MI 48072

THIS CERTIFICATE IS ISSUED AS A MATTER OF INFORMATION ONLY AND CONFERS NO RIGHTS UPON THE CERTIFICATE HOLDER. THIS CERTIFICATE OF INSURANCE DOES NOT AFFIRMATIVELY OR NEGATIVELY AMEND, EXTEND, OR ALTER THE COVERAGE AFFORDED BY THE INSURANCE POLICIES BELOW.

INSURERS AFFORDING COVERAGE

INSURER A - Prime Insurance Company

INSURER B

INSURER C - Company #12568

COVERAGES

"LIMITS SHOWN ARE THOSE IN EFFECT AS OF POLICY INCEPTION"

777401

The policies of insurance listed below have been issued to the insured named above for the policy indicated. Notwithstanding any requirement, term or condition of any contract or other document with respect to which this certificate may be issued or may pertain, the insurance afforded by the policies described herein is subject to all the terms, exclusions and conditions of such policies. Aggregate limits shown may have been reduced by paid claims.

TYPE OF INSURANCE	POLICY NUMBER	POLICY EFFECTIVE DATE (MM/DD/YY)	POLICY EXPIRATION DATE (MM/DD/YY)	LIMITS
<input checked="" type="checkbox"/> Commercial Liability	SC25043756	04/29/2025	04/29/2026	\$50,000 Per Person \$100,000 Per Accident \$300,000 Policy Aggregate
<input checked="" type="checkbox"/> Claims Made				
<input checked="" type="checkbox"/> Exclude Products				
<input checked="" type="checkbox"/> Exclude Completed Operations				
<input type="checkbox"/> Commercial Auto Liability				
<input type="checkbox"/> Any Auto				
<input type="checkbox"/> All Owned Autos				
<input type="checkbox"/> Scheduled Autos				
<input type="checkbox"/> Hired Autos				
<input type="checkbox"/> Non-Owned Autos				
<input type="checkbox"/> Drive Away				
<input type="checkbox"/> Specifically Described Autos				
<input type="checkbox"/> Commercial Garage Liability				
<input type="checkbox"/> G.K.L.L.				
<input type="checkbox"/> O.T.R.P.D.				
<input type="checkbox"/> D.O.C.				
<input type="checkbox"/> Cargo				
<input type="checkbox"/> On Hook				
<input type="checkbox"/> Contractual Liability Indemnification				
<input type="checkbox"/> Wrongful Repossession				
<input type="checkbox"/> Exclude Completed Operations				
<input type="checkbox"/> Exclude Products				
<input type="checkbox"/> Claims Made				
<input type="checkbox"/> Excess Liability				
<input type="checkbox"/> Claims Made				

OTHER

DESCRIPTION OF OPERATION/LOCATIONS/VEHICLES/EXCLUSIONS ADDED BY ENDORSEMENT/SPECIAL PROVISIONS

Coverage is limited to only insured activities or operations identified in the Policy. Primary & Non-Contributory Endorsement, Additional Insured Endorsement - Scheduled, Gunsmiths - Gun Buyback - Per Participant

☒ **CERTIFICATE HOLDER** ☐ **ADDITIONAL INSURED** ☐ **LOSS PAYEE** ☐ **WAIVER OF SUBROGATION** ☐ **PRIMARY AND NON-CONTRIBUTORY**

City of Berkeley, including all elected and appointed officials,
all employees, and volunteers, all boards, commissions, and/or authorities,
and they are board members, employees, and volunteers
3338 Coolidge Hwy
Berkeley, MI 48072

SHOULD ANY OF THE ABOVE-DESCRIBED POLICIES BE CANCELLED BEFORE THE STATED EXPIRATION DATE OR BE OTHERWISE AMENDED, THE CERTIFICATE HOLDER MAY NOT RECEIVE WRITTEN NOTICE. THE INSURER AND ITS AGENTS AND REPRESENTATIVES HAVE NO OBLIGATION OR LIABILITY OF ANY KIND TO A CERTIFICATE HOLDER WHO RELIES ON THE INFORMATION PROVIDED BY THIS CERTIFICATE.

AUTHORIZED REPRESENTATIVE



June 2, 2025 City Council Meeting

Moved by Councilmember _____ and seconded by Councilmember
_____ to approve a dedication ceremony in honor of former Mayor Maybelle Fraser,
and the purchase of a plaque to be placed on the City gazebo in her honor

Ayes:

Nays:

Absent:

Motion:

MEMORANDUM

To: Mayor Dean and City Council
From: Crystal VanVleck, City Manager
Date: *June 2, 2025*
Subject: Former Mayor, Mabelle Fraser Dedication

Madam Mayor and Members of City Council,

Background

Former Mayor, Maybelle Fraser, served on City Council from 1989 through 1993, and served as the City's first female mayor in 1995 for one term. She was instrumental in the construction of the gazebo that sits outside City Hall.

Summary

- To celebrate the hard work and commitment she put into building the gazebo, we would like to hold a dedication ceremony at the Gazebo in her honor.
- The Berkley Historical Museum will host an exhibit featuring former Mayor Fraser, and the City will place a plaque on the gazebo in dedication to her.
- The amount of the plaque is \$1,060, and will match the existing plaques on the gazebo.

Recommendation

Approve the dedication ceremony and the purchase of a plaque to be placed on the gazebo.

June 2, 2025 City Council Meeting

Moved by Councilmember _____ and seconded by Councilmember
_____ to review the Quarter 1 Strategic Framework report.

Ayes:

Nays:

Absent:

Motion:

MEMORANDUM

To: Mayor Dean and City Council
From: Crystal VanVleck, City Manager, Charlaine Stevenson, Assistant to the City Manager
Date: *June 2, 2025*
Subject: Strategic Framework

Madam Mayor and Members of City Council,

Background

Throughout 2024 and 2025, City Council and department directors met in work session to clearly define the priorities of the City for the next two years. This process resulted in the 2025-2027 Strategic Framework. This framework serves as a roadmap for the City of Berkley, prioritizing our human and financial resources around the achievement of the objectives.

Summary

Strategic Framework Quarterly Reports are an opportunity to share the City's progress in achieving these objectives, highlighting the hard work of staff in moving the City forward in the direction of our established priorities.

Recommendation

There is no action necessary, as this is a report and presentation.

CITY OF BERKLEY

Strategic Framework Quarterly Report

Q1 2025





INTRODUCTION

Throughout 2024 and 2025, City Council and department directors met in work sessions to clearly define priority areas and the objectives that will help us reach them. Based on feedback from the community, discussions in the work sessions, and information from staff, the Strategic Framework was created as a plan for the next two years.

The Strategic Framework serves as a roadmap for the City of Berkley, helping us to define our goals and hone our focus on how to achieve them. It's a living document that helps us make decisions about how to allocate our resources for the best benefit of the community as well as track and report our progress.

The Strategic Framework Reports are an opportunity to highlight the activities that the City of Berkley has carried out in the previous three months in an effort to work toward our established objectives.



VISION, MISSION, VALUES

Mission

Berkley will strive to enhance economic vitality, preserve neighborhoods, and foster progress to implement the City's vision and values.

Vision

Berkley will be a thriving 21st Century municipality, rooted in strong neighborhoods and a walkable design, supported by a caring community that helps every resident, business, and visitor to flourish.

Values

Caring, Innovative, Welcoming, Active



PRIORITY AREAS

Priority Area 1:

Organizational Effectiveness

Priority Area 2:

Economic Sustainability

Priority Area 3:

Community Assets

Priority Area 4:

Open Communications

Priority Area 5:

Fiscal Stewardship



QUARTERLY REPORT

Priority Area 1: Organizational Effectiveness

Objective: Cultivate a strong organizational culture

- Planning is progressing for the summer staff picnic.
- Human Resources and the City Manager are planning a citywide Leadership/HR Update to introduce and engage city staff in the strategic framework, as well as the future of Human Resources and upcoming Open Enrollment period.
- Human Resources has engaged Oakland County in broadening our citywide training opportunities, including access to online training resources.
- City Clerk is ahead of the curve, and has incorporated annual evaluations of the Deputy Clerk.

Objective: Optimize organization, structure, staffing, policies/procedures, and level of service

- The City Manager's office has identified all existing policies citywide and within each department. After evaluating each policy, Leadership will make updates as needed and create new ones where gaps exist.
- The Clerk's Office has created a Standard Operating Procedure Manual shared with Human Resources to document procedures done regularly.
- Human Resources created and assisted with a plethora of policies, including but not limited to Drug & Alcohol, Use of City Pool Vehicles,

Social Media, and Media and Communications.

- All human resources functions, including FMLA, workers' compensation, benefits, recruitment, disability, and COBRA, have been officially handed off to our Human Resources Department.
- The Clerk's Office staff has been cross-trained with the Treasury Department on cash-receipting.
- Parks and Recreation, Facilities, and Public Works have been consolidated, creating the Department of Public Services. This has created shared resources and responsibilities of multiple departments that consistently work together to deliver various services.
- The Treasury and Finance Department has consolidated shared responsibilities with the departments and created more efficiency.
- The City Manager's Office and the Clerk's Office have worked together to complete Phase 1 of the Boards and Commission Policy changes.
- Human Resources created new and updated existing paperwork implementation, including ADN Registration form, Background Check Authorization, and Personal Information forms.
- Human Resources has implemented interview procedures, including standards for interviewing applicants.
- Human Resources conducted an audit of employee I-9 forms.

Objective: Foster stronger relationships with community and regional partners

- The Communications Department attended two SEMCOG Communicators Network meetings to build relationships with SEMCOG and other communities' communication departments.

- The Clerk's Office entered into a partnership with Oakland County and other neighboring communities to create a regional Early Voting Center.
- The Downtown Development Authority (DDA) attended the Michigan Downtown Association (MDA) workshop and all Main Street Oakland County (MSOC) Managers' Meetings. The DDA was able to talk to various vendors and network with other communities about their initiatives.
- The City Manager and Assistant to the City Manager have both attended several Oakland County City Manager Association luncheons.

Objective: Identify and implement innovative operational solutions

- Community Development has made all permitting, except those that require a detailed plan review, available online.
- The Finance Department has created and updated templates for daily tasks to standardize all processes.
- Public Safety has implemented the parking permit portal, and is almost a paperless department.
- The Clerk's Office has scanned all contracts and is migrating all documents to digital files on the internal server.
- The IT Department and the Clerk's Office have started to scan everything that is within the retention schedule.



QUARTERLY REPORT

Priority Area 2: Economic Sustainability

Objective: Identify, prioritize, and deploy economic development tools

- Community Development and the City Manager met with the Michigan Economic Development Corporation (MEDC) and Oakland County to discuss various tools and opportunities for development.
- Community Development has presented the PILOT Workforce ordinance to the City Council, which provides an additional economic development tool to the city.
- Community Development has started conversations with Carlisle Wortman on policies for payment in lieu of parking and leasing municipal spaces.

Objective: Expand economic recruitment and retention efforts strengthening the City's economic portfolio

- Community Development has met with Redevelopment Ready Communities (RRC) to coordinate the completion of our marketing plan, which is a requirement of RRC.

Objective: Implement Master Plan updates and development process improvements

- Community Development has finished the Zoning Ordinance, a cover-to-cover reexamination of all zoning regulations.

- Community Development is actively working on completing the Master Plan Implementation Matrix. The majority of it is complete, with several tasks underway and five on hold due to funding issues or a longer time horizon.



QUARTERLY REPORT

Priority Area 3: Community Assets

Objective: Complete comprehensive asset needs inventory and management plan

- Facilities completed an assessment of all municipal buildings in February 2025. The assessment provides an analysis of our long-term maintenance needs and lifecycles of our facilities, and will guide us in a proactive approach to strategically invest in our assets. Future capital needs will be reflected in the City's Capital Improvement Plan.

Objective: Develop sustainable asset investment strategy that supports funding for the City's capital improvement plan

- Implemented an overhaul of the City's Capital Improvement Plan process to ensure a thorough, well-thought-out long-term plan to address our capital needs.
- Scheduled to set aside \$315,000 this Fiscal Year into the new Public Improvement Fund, which was created to strategically invest in and fund the City's capital needs.

Objective: Improve community resiliency

- Community Development worked on the engineering Design Standards, which include a comprehensive guide to developing and redeveloping property. This focuses on non-residential and large-scale residential projects. These standards include current and future standards from

Oakland County, MDOT, and other regulatory agencies.

Objective: Improve facility/staff security and safety

- The Clerk's Office has created a comprehensive election security plan with Emergency Manager Jack Blanchard and the Berkley School District.
- Public Safety conducts monthly police/fire/EMS training for personnel.
- Money has been included in the FY 2025-26 recommended budget to update security measures at City Hall and the Community Center, including cameras and expansion of the card access system.



QUARTERLY REPORT

Priority Area 4: Open Communications

Objective: Identify community audiences and most effective City “voice(s)”

- The Communication Department is developing a communications survey to identify community members’ preferred communication channels.

Objective: Identify and implement priority communication strategies within human and financial resources

- The Clerk's Office has developed an internal communication platform to monitor workflow and identify communications regarding information from all departments that may be conveyed to the public.
- The Communications Department has launched the new website and fixed all bugs.
- Parks and Recreation and the Library worked with the Communications Department to release the second program guide for Spring/Summer, providing residents with important information regarding events and programs.
- The Communications Department continues to provide robust information to the community via social media, email, specialized mailers, and the City website, including Council meeting recaps, changes to yard waste/chipper service, water main breaks, and city-hosted events like Winter Fest.

- We have utilized the new website and social media to advertise job openings and the audit report with consistent branding.

Objective: Foster meaningful community engagement and genuine relationships

- The City Manager's office and Clerk's office facilitated two Board and Commission meetings with our AdHoc committee and one with Board and Commission chairs to manage policy updates. Attendees were able to provide feedback on policies and procedures to the AdHoc Committee and City staff.
- City staff started meetings with business owners and residents to aid in planning and ramping up the Dorothea Pilot Pocket Park.



QUARTERLY REPORT

Priority Area 5: Fiscal Stewardship

Objective: Clearly define and articulate the City's long-term fiscal realities

- The Finance Department and City Manager's office recommended a 3-year budget (Fiscal Year 2025/26 – Fiscal Year 2027/28), including the 6-year Capital Improvement Plan forecast for the city. This year's budget document was transformed into a more user-friendly version that tells the story of the City's budget forecast, through the hard work of the Communications Department and Finance Department.

Objective: Identify additional funding strategies and partnerships

- Submitted requests to federal and state agencies for four projects for the 2025–2026 fiscal year.
- Submitted grant applications for the Community Museum Grant Program and the DTE Energy Foundation Tree Planting Grant.
- The City Clerk's Office conducted an audit of the Fee Schedule and worked to create a new, efficient format. The Clerk's office is now the keeper of the fee schedule and will maintain all updated versions.

Objective: Investigate new shared services opportunities

- The Clerk's Office attends quarterly meetings with the Oakland County Clerks Association, creating potential partnership opportunities.

- The Finance Department is working with BS&A to automate the AP approval process and streamline Bond and Escrows.

Objective: Update financial policies and procedures

- The Finance Department is working with BS&A to automate the AP approval process and streamline Bond and Escrows.